



JOHN W. BARTLETT, CHAIR

BOARD MEETING AGENDA

March 13, 2023

10:30 AM

Note: This meeting is being held **in-person** at NJTPA offices at One Newark Center (1085 Raymond Blvd.), 17th Floor, Newark, NJ 07102. Public comments can be made in-person or via a special Zoom meeting link which requires registration in advance at NJTPA@NJTPA.org by Friday March 10, 2023.

- A. Open Public Meetings Act Compliance
 - B. Salute to the Flag
 - C. Roll Call
 - D. Approval of Minutes
 - E. Chair's Remarks
 - F. Executive Director's Report
 - G. Presentation: Pathways to Zero: Decarbonizing the Transportation Sector – Dr. Jesse Jenkins, Assistant Professor of Mechanical and Aerospace Engineering, the Andlinger Center for Energy and the Environment, Princeton University
 - H. Committee Reports/Action Items*
- Project Prioritization – Commissioner John P. Kelly, Chair
 - 1. Minor Amendment to the FY 2022 – 2025 Transportation Improvement Program to Add the Boylan Terrace Neighborhood Pedestrian Connection Project as Requested by the Borough of Bernardsville (Attachments H.1.a., H.1.b.)
 - 2. Minor Amendment to the FY 2022 – 2025 Transportation Improvement Program to Add the NJTPA Carbon Reduction Program as Requested by the North Jersey Transportation Planning Authority (Attachments H.2.a., H.2.b.)

* Following the Committee Reports, there will be an opportunity for public comment prior to any Action Item vote.

3. Minor Amendments to the FY 2022 – 2025 Transportation Improvement Program to Add Federal Funds to Three Projects as Requested by the New Jersey Department of Transportation (Attachments H.3.a., H.3.b.)
 4. Approval of Projects for the FY 2022 Local Safety Program and High Risk Rural Roads Program (Attachments H.4.a., H.4.b., H.4.c.)
 5. Approval of the Initial Financial Plan for the Route 7, Kearny Drainage Improvements Project (Attachments H.5.a., H.5.b., H.5.c.)
- Planning and Economic Development – Commissioner Charles Kenny, Chair
 6. Approval of FY 2024 Unified Planning Work Program (Attachments H.6.a., H.6.b.)
 - Freight Initiatives – Commissioner Jason Sarnoski, Chair
 - I. Public Participation – Members of the public who wish to comment via Zoom must register in advance at NJTPA@NJTPA.org by Friday March 10, 2023. A link will be provided.
 - J. Time and Place of Next Meeting: The next meeting of the NJTPA will be held in-person on May 8, 2023, 10:30 a.m. at the NJTPA, One Newark Center (1085 Raymond Blvd.), 17th Floor, Newark, NJ 07102.
 - K. Adjournment



JOHN W. BARTLETT, CHAIR

Board Meeting Minutes

January 9, 2023

A. Open Public Meetings Act Compliance

Chair John W. Bartlett, Passaic County, called the meeting to order at 10:30 a.m. Beverly Morris, Central Staff, reported that, in accordance with the Open Public Meetings Act, Chapter 231, P.L.1975, adequate notice of this meeting was forwarded to the *Star Ledger*, the *Asbury Park Press*, the *Courier News*, the *Daily Record*, the *Home News Tribune*, the *Jersey Journal*, *New Jersey Herald*, and *The Record*. It was posted at the Essex County Hall of Records in Newark, New Jersey.

B. Roll Call

Ms. Morris called the roll. Sixteen voting members were present. (Attachment 1).

C. Approval of Minutes

A motion to approve the minutes of the November 14, 2022 meeting was made by Ocean County, seconded by Warren County and carried unanimously.

D. Chairman's Remarks

Chair Bartlett welcomed Morris County Commissioner Stephen H. Shaw to the Board as the County's voting member. Commissioner Shaw replaced Commissioner Kathy DeFillippo, who retired at the end of the year. The Chair encouraged all to see the latest edition of *InTransition* magazine, which features articles on microtransit, — the subject of today's Board presentation — alternative fuel vehicles, and how the pandemic continues to affect public transit agencies.

The Chair noted that the work being done by the NJTPA is guided by its long-range plan, Plan 2050, which was approved in 2021 and is scheduled to be updated in 2025. Work on that update, he noted, has already commenced.

The Chair said the NJTPA continues several municipally focused programs, which staff recently promoted at the New Jersey State League of Municipalities conference. He said, while most of the NJTPA's funding goes directly to the subregions and partner agencies, there are several

technical assistance programs open to municipalities. This year, he said, the NJTPA is working with Harrison under the Planning for Emerging Centers Program; seven municipalities are participating in the latest cycle of the Complete Streets Technical Assistance Program, and Vibrant Places Program projects are being conducted in Lambertville and the Village of Asbury.

Chair Bartlett said the NJTPA booth at the 2022 League of Municipalities conference helped to introduce the NJTPA to many more municipal officials who may not normally work with the agency. Staff was also able to share information about federal funding opportunities under the Infrastructure Investment and Jobs Act (IIJA). He said he attended the conference, and Commissioner Charles Kenny, Middlesex County, also joined staff at the NJTPA booth there. The Chair encouraged all to take part in such gatherings as a way to represent the NJTPA and become more familiar with staff and the outreach materials they use to connect with the subregions and municipalities. He reminded all to refer to the [NJTPA IIJA webpage](#) for updates on available grant opportunities.

The Chair said the NJTPA will return to fully in-person Board meetings starting in March. He said a survey of Board members indicated consensus that there is value in meeting face-to-face, because it provides more opportunities for Board members and staff to collaborate and share ideas, and they allow them to interact with members of the public, who come out to speak about action items and other matters. He said Board meetings will continue to be livestreamed on the NJTPA website and YouTube channel. Standing committee meetings will continue to be held virtually.

E. Executive Director's Report

NJTPA Executive Director David W. Behrend said he is optimistic about the new year and the opportunities it brings to the NJTPA, including continued progress in implementing the priorities of Plan 2050, such as maintaining and improving roads, bridges, and rail lines; supporting complete streets and improving safety; encouraging alternative transportation modes; and more. He said, in doing all this work, Central Staff will build on long-standing, cooperative relationships with New Jersey Department of Transportation (NJDOT), NJ TRANSIT, the Port Authority of New York & New Jersey (PANYNJ), the Governor's Authorities Unit, and the subregions. He said these working relationships should enable the region to take full advantage of federal funding programs and grants under the IIJA, which covers a wide range of infrastructure needs. Mr. Behrend said we are already seeing benefits from funding under the law, such as the rehabilitation of transit stations, planning projects such as exploring a ferry service and support for expanding the electric charging network. Increased funding has also supported critical road and bridge projects and led to progress in replacing the Portal Bridge and building new Hudson River rail tunnels.

Mr. Behrend noted that, according to figures just received from NJDOT, in the past fiscal year, the region authorized \$29.4 million in federal Highway Safety Improvement Program (HSIP) funding for priority safety projects in the Local Safety Program. He said it is the highest amount ever authorized in one fiscal year for the region in that program. He said additional projects in the Local Safety and High Risk Rural Roads programs, funded by HSIP, will be going before the Project Prioritization Committee in February and the Board in March.

Mr. Behrend said in 2023, the NJTPA will help additional subregions apply for funding under the federal Safe Streets for All Program. He said at least seven NJTPA subregions applied for funding from the current round of the program and an award announcement from the US Department of Transportation is expected shortly. He said, to enable other subregions to apply for future rounds of these grants and to bolster their safety planning activities, the NJTPA is offering to assist them in developing Local Safety Action Plans, which are a pre-requisite for seeking federal funding for implementation. He noted that this will be the subject of a Regional Transportation Advisory Committee (RTAC) teleconference on January 17. These Safety Action Plans will consider capital improvements, education, and enforcement. Mr. Behrend said, in addition to providing eligibility for federal grant awards, the plans will be valuable for identifying and developing strategies to address local safety needs. He urged all subregions to consider participating.

Mr. Behrend said the Unified Planning Work Program (UPWP) for Fiscal Year (FY) 2024, which begins July 1, guides the work of Central Staff, the subregions, and Transportation Management Associations. A [link](#) to the draft work program was distributed to all members for review. Comments are due by January 13, after which the work program will go before the Planning and Economic Development Committee in February and to the Board for approval in March.

Mr. Behrend highlighted some interesting new activities in the work program in addition to mandated and ongoing work: updates of freight industry forecasts and the Regional Capital Investment Strategy; initial public outreach work for the next long-range plan; and working with the New York Metropolitan Transportation Council to prepare a new regional household travel survey.

Mr. Behrend reported that, on November 15, the NJTPA hosted a meeting of the Multi-State Freight Working Group, a subset of the Metropolitan Area Planning (MAP) Forum, which includes representatives of 10 metropolitan planning organizations and councils of government from New Jersey, New York, Connecticut, and Pennsylvania. The topic was Effective Practices for Enhancing Last Mile Workforce Access in the freight-related sector. Also, the NJTPA participated in the MAP Forum's annual meeting on December 2. Topics included improvements on the Northeast Corridor and New York City's central business district congestion pricing plan.

Mr. Behrend said the NJTPA will participate in the annual NJ TransAction conference in April and the New Jersey Association of Counties conference in May — two more opportunities to promote the NJTPA's various projects and programs.

F. Presentation

Eric Gardiner, East Coast Partnerships Director for Via Transportation, Inc., said the New York City-based transportation technology company works with more than 600 public entities and facilitates 2.5 million shared rides each month. Jersey City partnered with Via to launch a microtransit service that uses Via's turnkey solution, which includes technology, drivers and vehicles. The app-based service fills gaps where fixed-route network transit (bus routes, for

example) is limited and last-mile options are needed. Mr. Gardiner said, since Via Jersey City launched in 2020, it has provided more than 1.25 million rides averaging about 2,000 per day. He said more than half of users report a household income of less than \$50,000, while 88 percent identify as minorities, 58 percent as women, and 68 are under the age of 35. Michael Manzella, Jersey City's Director of Transportation, said Via is serving the exact population it was expected to, with ridership growing about 23 percent from 2021 to 2022 and approaching five passenger rides per hour.

Mr. Gardiner said the program is helping Jersey City to improve safety by reducing automobiles on the road. Chair Bartlett said the fact that people are able to access the service for the first- and last-mile destinations is probably reducing pedestrian crossings at roadways and increasing pedestrian safety.

Arjun Janakiram, Via's funding policy researcher, said there are several federal funding programs under the IIJA that communities could use to create microtransit programs, including, the Carbon Reduction Program, the Congestion Relief Program, the Rural Transportation Program and the Advanced Technologies Program.

A wide-ranging Q&A session ensued.

G. Committee Reports/Action Items

Project Prioritization – Commissioner John P. Kelly, Chair

Commissioner Kelly said the Committee considered five actions at the joint committee meeting in December, two of which were on the agenda. The first is a set of minor amendments to the current Transportation Improvement Program (TIP) to add federal funds to two projects. For the Route 80, Bridges over Howard Boulevard project in Morris County, one amendment will move \$14 million from Federal FY 2024 to 2023, and add \$16 million, for a revised total of \$44 million for Construction in 2023. The funding source will be changed from National Highway Performance Program funds to Bridge Formula Program funds. The additional cost is due to the increased price of materials and changes needed to meet storm water requirements. The Commissioner said the amendment will also add \$49.5 million to the NJTPA Pavement Preservation Program in FY 2023, for a new total of \$68.26 million. The additional funds are required to advance 10 pavement preservation projects in the region, which are anticipated to be authorized in the current fiscal year.

The Commissioner said the next action item is a set of minor amendments to add two local projects as requested by Morris and Somerset counties. He said these projects have completed Concept Development and are ready to be programmed into the current TIP with federal funding. The Route 24, Eastbound Ramp to Columbia Turnpike project in Morris County will be allocated \$17 million, and the Great Road, Bridge over Bedens Brook project in Somerset County will be allocated \$6.9 million. Both projects will be funded from the NJTPA Future Projects line item, covering the Preliminary Engineering through Construction phases of work from FY 2025 to 2028.

Commissioner Kelly said, also at the meeting, the Committee considered two modifications to the TIP. The first adds federal funds to the Kapkowski Road – North Avenue East Improvement Project, as requested by the City of Elizabeth. This modification programs federal funds and updates the project schedule. The revised total for the project is \$18.4 million through FY 2027. He said the other modification adds funds to three projects as requested by NJDOT, as follows:

- For the Route 280, Westbound Ramp over 1st & Orange Streets, Newark Subway & NJ TRANSIT project in Essex County, \$16 million in programmed funds was moved from FY 2024 to 2023 and \$11.6 million was added for a revised total of \$42.7 million for Construction in the current fiscal year. The cost increase is based on updated construction engineering, construction inspection, and utility estimates.
- For the Route 440, Route 95 to Kreil Street project in Middlesex County, \$4.8 million was added for Preliminary Engineering in the current fiscal year, increasing the total amount to \$7.8 million. The project’s complexity has increased the estimated cost of this phase of work.
- For the statewide Drainage Rehabilitation & Improvements Program, \$11.2 million was added for Engineering and Construction in FY 2023, for a new total of \$22.45 million.

The Commissioner said the Committee approved the modifications, and no further action is required.

The Commissioner said the Committee also considered the 2022 Financial Plan update for the Replacement of Bridge S-32 on Rumson Road over the Shrewsbury River project in Monmouth County. Under federal law, financial plans are required for projects costing between \$100 million and \$500 million. The project is anticipated to cost \$131 million. As of August 29, the project had expended \$14.1 million. Construction began in January 2021 and will continue through May 2025. There has been no change in the construction cost since last year. As with the TIP modifications, no further action is required on the financial plan update.

Action Item 1: Minor Amendments to the FY 2022-2025 Transportation Improvement Program to Add Federal Funds to the Route 80, Bridges over Howard Boulevard in Morris County and to the Pavement Preservation, NJTPA Regional Program as Requested by the New Jersey Department of Transportation (Attachment 2)

A motion to approve the resolution was made by NJDOT, seconded by Bergen County and carried unanimously.

Action Item 2: Minor Amendments to the FY 2022-2025 Transportation Improvement Program to Add Two Local Projects as Requested by Morris County and Somerset County (Attachment 3)

- Route 24, East Bound Ramp to Columbia Turnpike (CR 510), Morris County
- Great Road (CR 601), Bridge over Bedens Brook (D0105), Somerset County

A motion to approve the resolution was made by Monmouth County, seconded by Essex County and carried unanimously.

Planning and Economic Development – Commissioner Charles Kenny, Chair

Commissioner Kenny said the Committee considered one action item at the December joint meeting, approval of the NJTPA Roadway Safety Performance Measure Targets for 2023. He explained that the region must adopt regional roadway safety targets that meet federal requirements while also advancing the safety priorities described in Plan 2050. This year, for the first time, the NJTPA is proposing roadway safety targets specific to the region, rather than adopting the statewide targets set by NJDOT. He noted that the Delaware Valley Regional Planning Commission has already adopted targets specific to its region, which includes parts of New Jersey.

The Commissioner said the proposed NJTPA region targets are evidence-based and were set at levels to put the region on a path to reduce fatalities and serious injuries caused by crashes to zero by 2050. The federal government requires targets for five roadway safety performance measures. They are the five-year rolling averages of:

- The number of fatalities
- The rate of fatalities per 100 million vehicle miles travelled
- The number and rate of serious injuries, and
- The number of non-motorized fatalities and serious injuries, which refers to cyclists and pedestrians

Commissioner Kenny said target setting this year is particularly challenging because the data reflect increases in all five of these categories in the region and all of New Jersey since the beginning of the COVID-19 pandemic. A number of NJTPA projects and programs are intended to help the region meet these targets, including: the Local Safety and High Risk Rural Roads programs, Complete Streets Technical Assistance, Travel Demand Management, and others.

The Commissioner reported that the meeting also featured a presentation by Sam Schroeder, a Supervising Planner for Hudson County, on efforts to develop the eight-mile Hackensack River Greenway, which would tie into several other planned trails. Mr. Schroeder briefed the committees on the Hackensack River Greenway Working Group's efforts to identify and facilitate improvements and connections along the Greenway. The working group includes representatives from Hudson County, Bayonne, Jersey City, Secaucus, the NJTPA, the New Jersey Sports and Exhibition Authority, Skyway Park Conservancy and the Hackensack Riverkeeper.

Commissioner Kenny said that the Committee was also briefed on a consultant effort planned for the coming fiscal year, which will assist subregions in developing Local Safety Action Plans, which are a requirement for accessing federal Safe Streets and Roads for All implementation funding. As Mr. Behrend noted, this is one of several initiatives in the draft FY 2024 work program.

Action Item: NJTPA Roadway Safety Performance Measure Targets for 2023 (Attachment 4)

A motion to approve the resolution was made by Union County and seconded by the Citizens' Representative.

Andy Weiss, a member of the public, commented on the action item. Mr. Weiss said all the county and state roads are dangerous, and local municipalities and NJDOT are unresponsive to residents' complaints about it. He asked the Board to "work on this."

Chair Bartlett asked how the targets are used, and what role they play in the project funding decisions that the Board makes. Jeff Perlman, Central Staff, said setting NJTPA region-specific targets is consistent with the safety elements of Plan 2050 and serves as policy guidance for a number of subregional programs, such as the Local Safety Action Plans.

In answer to a question from Commissioner Jason Sarnoski, Warren County, Mr. Perlman said the NJTPA targets are slightly more stringent than those set by NJDOT.

Andy Swords, NJDOT, noted that the Department sets its safety targets to meet federal requirements, and it is very much a data-driven process. He said some challenges have been causing difficulty, such as changes in serious injury reporting as well as the increase in fatalities. He noted that FY 2022 saw a record amount of Highway Safety Improvement Program funds – approximately \$93 million – supporting the Department's safety efforts. Mr. Swords said NJDOT has also set a vision toward zero fatalities by 2050. He noted that NJDOT safety planning is conducted in collaboration with partners at the NJTPA and others.

Mr. Behrend said NJTPA targets are also based on very specific requirements from the federal government that have to do with five-year rolling averages. He said part of the reason the agency is choosing to set its own targets is that many NJTPA subregions are making their own Vision Zero efforts to contribute toward improving safety in the region.

Following the comments, the motion was carried unanimously.

Freight Initiatives – Commissioner Jason Sarnoski, Chair

Commissioner Sarnoski said the Committee heard from three speakers at its December meeting, which featured its fifth annual New Jersey Industrial Real Estate Update.

The Commissioner said the first speaker was William Waxman, Vice Chair at Cushman and Wakefield, who said there is still a huge demand for warehouse space, and he estimates that about 140 million square feet are being built in the New York-New Jersey-Lehigh Valley area. Over the past three years, demand has been driven by e-commerce companies looking to build their last-mile, quick delivery services. This has caused industrial properties pricing to increase by up to 25 percent and forced some regional hubs to move further south and into Pennsylvania and Delaware, where rents are much cheaper. However, Mr. Waxman said, the tenant mix has

begun to shift more towards retail companies, third party logistics firms, health care products, and food and beverage companies. He said that the vacancy rate has begun to grow a little, which has slowed the increase in rental fees. However, Mr. Waxman noted, the demand for cold storage has grown and needs to be met.

Commissioner Sarnoski said the next speaker was Steven Beyda, Senior Vice President of Acquisitions at Woodmont Industrial Partners, who spoke about trends in converting office space to industrial uses within North and Central New Jersey, mostly in former office parks in Bergen, Middlesex, Morris and Somerset counties. With statewide office space vacancies at about 20 percent, he said there will be other conversions as developers reconsider the best use of properties. Mr. Beyda noted that office buildings tend to be concentrated in locations with strong labor and can often meet the transportation requirements for industrial uses.

The Commissioner said the third speaker was Jesse Harty, Senior Vice President and Market Officer for New Jersey and New York at Prologis, who spoke about how developers will need to get more creative to meet growing demand for space. He noted that site constraints and the cost of land in New York City have led to the creation of multi-story industrial developments, and it remains to be seen if this strategy comes to New Jersey.

Commissioner Sarnoski said, during the meeting, NJTPA freight staff provided an update on studies that are underway, including the Freight Rail Grade Crossing Assessment Update and the Freight Concept Development Program studies in Somerset and Morris counties. Among the Committee member reports at the meeting, NJDOT said senior leadership is reviewing Rail Freight Assistance Program award recommendations, as well as the Statewide Freight Plan Update. The PANYNJ reported that, despite the record increase in container traffic at the port, ships are facing less waiting time, reflecting the ability of the freight network to move containers despite congestion.

H) Public Participation

David Peter Alan, a member of the Senior Citizens and Disabled Residents Transportation Advisory Committee (SCDRTAC) at NJ TRANSIT, asked the NJTPA to intercede on behalf of the committee, which he said is facing restrictions on its activities through a proposed change to the New Jersey Administrative Code. He said the Committee gives advice within and outside NJ TRANSIT on issues concerning transportation for seniors and persons living with disabilities. He said its purview has been broad, including ADA accommodation, accessibility for fixed-route transit, funding, connectivity, Access Link, and other community-run transportation.

Mr. Weiss said the November 30 virtual Hudson County Safety Improvement Project meeting unfairly restricted public input to a small chat box. He called on the NJTPA to investigate what happened and facilitate a new meeting. Concerning the Via presentation, Mr. Weiss said Via Jersey City replaced a great plan for countywide bus rapid transit (BRT) that included dedicated bus lanes. He said Via does not have dedicated lanes in Jersey City, gets stuck in slow-moving car traffic, only covers half of the city and closes at 10:00 p.m. He added that high-frequency BRT is very much needed on Kennedy Boulevard.

Tim Sevenser, New Jersey Association of Rail Passengers, said the cost of the I-78 expansion in Jersey City has doubled to \$10.7 billion, and he questioned the sense of widening highways. He proposed the alternative of restoring service on several rail lines that have seen cuts in service. Mr. Sevenser also proposed the electrification of certain rail lines that still operate on diesel fuel.

Sally Gellert, Chair of Lackawanna Coalition and Vice Chair of SCDRTAC, expressed concern about the proliferation of warehouse development in the state. She also said that, while microtransit can provide useful connections, we should be wary of the privatization of public transit. Ms. Gellert asked for the NJTPA's support in allowing SCDRTAC to continue to talk about the needs of senior citizens and disabled riders. She said the committee is the sole place where they can interact with NJ TRANSIT representatives who sometimes attend their meetings. She said SCDRTAC serves as a good voice for its constituents.

Sutapa Bandyopadhyay, Federal Highway Administration, added a few points concerning federal grants that were mentioned in the Via presentation. She said the Carbon Reduction Program involves formula funding, and applications for the following discretionary programs are considered on a competitive basis: Congestion Relief Program, Rural Transportation Program, and the Advanced Transportation and Congestion Management Technologies Deployment Program (ATCMTD). She noted that New Jersey has not applied for ATCMTD funding for some time, and she encouraged all to look at the requirements for it. Ms. Bandyopadhyay said applicants for this program can partner with universities to submit proposals, which many funding sources do not allow.

I) Time and Place of Next Meeting

The next NJTPA Board meeting will be held on Monday, March 13 at the NJTPA office in Newark at 10:30 a.m.

J) Adjournment

At 12:07 p.m., a motion to adjourn was made by Ocean County, seconded by Union County and carried unanimously.

NORTH JERSEY TRANSPORTATION PLANNING AUTHORITY, INC.
Meeting of the Board of Trustees
Attendance Record: January 9, 2023

Subregion/Agency	Voting Members/Alternates	Staff & Others
Bergen County	Peter Botsolas	
Essex County	David Antonio	
Hudson County		Thomas Malavasi
Hunterdon County		Katherine Fullerton
Jersey City	Michael Manzella	Elias Guseman
Middlesex County	Hon. Charles Kenny Doug Greenfeld	Mike Dannemiller Khalid Anjum
Monmouth County	Teri O'Connor	Joseph Ettore Shilpa Bhojappa
Morris County	Hon. Stephen Shaw John Hayes	Dede Murray
Newark		
Ocean County	Hon. John Kelly John Ernst	Mark Jehnke
Passaic County	Hon. John Bartlett	Andras Holzmann Sal Presti Massiel Ferrara
Somerset County	Hon. Sara Sooy	Ken Wedeen
Sussex County	Tom Drabic	
Union County	Hon. Bette Jane Kowalski	Liza Betz
Warren County	Hon. Jason Sarnoski Dave Dech	Ryan Conklin
Office of the Governor	Dorian Smith	
NJDOT	Andy Swords	Eric Powers
NJ TRANSIT	Lou Millan	Jack Dean
Port Authority of New York & New Jersey (PANYNJ)		Mary K. Murphy
Citizen's Representative	Jared Rodriguez	

Other Attendees	
Neile Weissman	Complete George
Jack Kanarek	Dewberry
Sutapa Bandyopadhyay	FHWA
Bob Werkmeister	GPI

Approved January 9, 2023

Mark Baudermann	Hatch
Bakari Lee	McManimon, Scotland & Baumann, LLC
Tim Sevenser	NJ Association of Rail Passengers
RJ Palladino	NJ TRANSIT
Various members of Central Staff	NJTPA
Sally Gellert	Senior Citizens and Disabled Residents Transportation Advisory Committee (SCDRTAC)
David Peter Alan	SCDRTAC
Arjun Janakiram	Via
Eric Gardiner	Via
Sara-Jessica Dilks	Via
Andy Weiss	
Paul St. Onge	
Colleen Wilson	

**DRAFT RESOLUTION: MINOR AMENDMENTS TO THE FY 2022 – 2025
TRANSPORTATION IMPROVEMENT PROGRAM TO
ADD FEDERAL FUNDS TO THE ROUTE 80, BRIDGES
OVER HOWARD BOULEVARD IN MORRIS COUNTY
AND TO THE PAVEMENT PRESERVATION NJTPA
REGIONAL PROGRAM AS REQUESTED BY THE NEW
JERSEY DEPARTMENT OF TRANSPORTATION**

WHEREAS, the North Jersey Transportation Planning Authority, Inc. (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the NJTPA formally adopted the FY 2022 – 2025 Transportation Improvement Program (TIP) on September 13, 2021; and

WHEREAS, the NJTPA on September 24, 2012 approved a Memorandum of Understanding (MOU) among the NJTPA, the New Jersey Department of Transportation (NJDOT), and NJ TRANSIT on procedures to amend and modify the State Transportation Improvement Program and the NJTPA TIP; and

WHEREAS, the TIP may be revised any time; and

WHEREAS, according to the MOU, when federal funds above the specified threshold are added to a project or program in the TIP, and a new air quality conformity determination is not required, this constitutes a minor amendment; and

WHEREAS, the NJDOT has requested the addition of federal funds to the Route 80, Bridges over Howard Boulevard (CR 615) project (DB# 15351) in Morris County and to the Pavement Preservation NJTPA regional program (DB# X51B); and

WHEREAS, funds are available for these purposes; and

WHEREAS, this minor amendment is exempt from an air quality conformity determination and does not impact the current conformity determination; and

WHEREAS, Congestion Management Process requirements do not apply to these actions; and

WHEREAS, the FY 2022 – 2025 TIP conforms to the performance-based planning requirements of the Infrastructure Investment and Jobs Act and its predecessor legislation; and

WHEREAS, consistent with NJTPA public participation procedures, the NJTPA has provided opportunities for review of these actions; and

Approved January 9, 2023

WHEREAS, no action authorized by the NJTPA shall have force or effect until 10 days (Saturdays, Sundays and public holidays excepted) after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the specified amendment to the FY 2022 – 2025 Transportation Improvement Program.

BE IT FURTHER RESOLVED that a copy of this resolution is forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

**DRAFT RESOLUTION: MINOR AMENDMENTS TO THE FY 2022 – 2025
TRANSPORTATION IMPROVEMENT PROGRAM TO
ADD TWO LOCAL PROJECTS AS REQUESTED BY
MORRIS COUNTY AND SOMERSET COUNTY**

WHEREAS, the North Jersey Transportation Planning Authority, Inc. (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the NJTPA formally adopted the FY 2022 – 2025 Transportation Improvement Program (TIP) on September 13, 2021; and

WHEREAS, the NJTPA on September 24, 2012 approved a Memorandum of Understanding (MOU) among the NJTPA, the New Jersey Department of Transportation (NJDOT), and NJ TRANSIT on procedures to amend and modify the State Transportation Improvement Program and the NJTPA TIP; and

WHEREAS, the TIP may be revised any time; and

WHEREAS, according to the MOU when a project or program that uses federal funds is added to the TIP and a new air quality conformity determination is not required, this constitutes a minor amendment; and

WHEREAS, Morris County has requested the addition of the Route 24 Eastbound Ramp to Columbia Turnpike (CR 510) project (DB# 15433) to the FY2022 – 2025 TIP; and

WHEREAS, Somerset County has requested the addition of the Great Road (CR 601) Bridge over Bedens Brook (D0105) project (DB# N2008) to the FY2022 – 2025 TIP; and

WHEREAS, fiscal constraint is maintained because funds are available for these projects through the NJTPA Future Projects program line item (DB# N063); and

WHEREAS, these minor amendments are exempt from an air quality conformity determination and do not impact the current conformity determination; and

WHEREAS, Congestion Management Process requirements do not apply to these actions; and

WHEREAS, the FY 2022 – 2025 TIP conforms to the performance-based planning requirements of the Infrastructure Investment and Jobs Act and its predecessor legislation; and

WHEREAS, consistent with NJTPA public participation procedures, the NJTPA has provided opportunities for review of these actions; and

Approved January 9, 2023

WHEREAS, no action authorized by the NJTPA shall have force or effect until 10 days (Saturdays, Sundays and public holidays excepted) after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the specified amendment to the FY 2022 – 2025 Transportation Improvement Program.

BE IT FURTHER RESOLVED that a copy of this resolution is forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

DRAFT RESOLUTION: NJTPA ROADWAY SAFETY PERFORMANCE MEASURE TARGETS FOR 2023

WHEREAS, the North Jersey Transportation Planning Authority, Inc. (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, Title 23, Section 134 of the United States Code requires that each MPO undertake a transportation planning process that establishes and uses a performance-based approach to transportation decision-making to support national goals; that each MPO shall establish performance targets that address the performance measures, tracking progress toward attainment of critical outcomes for the region; and that each MPO shall integrate those targets into its planning documents and processes; and

WHEREAS, the Federal Highway Administration (FHWA) published the final rule, (23 CFR Part 490), on the Highway Safety Improvement Program (HSIP) on April 14, 2016, requiring the New Jersey Department of Transportation (NJDOT) to annually develop and adopt targets assessing the number of motor vehicle crash-related serious injuries and fatalities; serious injuries and fatalities per vehicle miles traveled (VMT); and the number of serious injuries and fatalities of non-motorized users; and

WHEREAS, on May 27, 2016, FHWA issued a final rule on metropolitan planning (23 CFR 450 & 771 and 49 CFR 613), under which MPOs must adopt safety targets by February 27, 2018 and each year thereafter; and

WHEREAS, the final rule also requires that MPOs coordinate with NJDOT to set performance targets for the specified measures and integrate those targets into their planning documents and processes; and

WHEREAS, the NJTPA coordinated with NJDOT, the New Jersey Division of Highway Traffic Safety, and the other MPOs in New Jersey on analyzing trends and developing appropriate safety targets; and

WHEREAS, the final rule on metropolitan planning states that MPOs have the option to (1) agree to program investments in support of NJDOT's targets, or (2) set its own quantifiable targets; and

WHEREAS, the NJTPA has developed quantifiable targets that serve as suitable benchmarks along a trajectory to achieving an elimination of traffic deaths and serious injuries by 2050; and

WHEREAS, these targets align with the planning goals and desired outcomes for the projects and programs of the NJTPA Long Range Transportation Plan and Transportation Improvement Program; and

Approved January 9, 2023

WHEREAS, no action authorized by the NJTPA shall have force or effect until 10 days (Saturdays, Sundays and public holidays excepted) after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval; and

NOW, THEREFORE, BE IT RESOLVED, that the NJTPA hereby adopts the attached targets for the roadway safety performance measures listed in National Performance Management Measures: Highway Safety Improvement Program (HSIP); Final Rule (23 CFR 490); and

BE IT FURTHER RESOLVED that a copy of this resolution be forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

Attachment H.1.

**DRAFT RESOLUTION: MINOR AMENDMENT TO THE FY 2022 – 2025
TRANSPORTATION IMPROVEMENT PROGRAM TO
ADD THE BOYLAN TERRACE NEIGHBORHOOD
PEDESTRIAN CONNECTION PROJECT AS REQUESTED
BY THE BOROUGH OF BERNARDSVILLE**

WHEREAS, the North Jersey Transportation Planning Authority (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the NJTPA formally adopted the FY 2022 – 2025 Transportation Improvement Program (TIP) on September 13, 2021; and

WHEREAS, the NJTPA on September 24, 2012 approved a Memorandum of Understanding (MOU) among the NJTPA, the New Jersey Department of Transportation (NJDOT), and NJ TRANSIT on procedures to amend and modify the State Transportation Improvement Program and the NJTPA TIP; and

WHEREAS, the TIP may be revised any time; and

WHEREAS, according to the MOU when a project or program that uses federal funds is added to the TIP and a new air quality conformity determination is not required, this constitutes a minor amendment; and

WHEREAS, the Borough of Bernardsville has requested the addition of the Boylan Terrace Neighborhood Connection project (DB# N2308) to the FY 2022 – 2025 TIP; and

WHEREAS, fiscal constraint is maintained because funds are available for this project through a Federal Fiscal Year 2022 Community Project Funding/Congressionally Directed Spending grant through the Federal Highway Infrastructure Programs; and

WHEREAS, this minor amendment is exempt from an air quality conformity determination and does not impact the current conformity determination; and

WHEREAS, Congestion Management Process requirements do not apply to these actions; and

WHEREAS, the FY 2022 – 2025 TIP conforms to the performance-based planning requirements of the Infrastructure Investment and Jobs Act and its predecessor legislation; and

WHEREAS, the NJTPA has provided opportunities for review of these actions consistent with NJTPA public participation procedures; and

WHEREAS, no action authorized by the NJTPA shall have force or effect until 10 days (Saturdays, Sundays and public holidays excepted) after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of

the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the specified amendment to the FY 2022 – 2025 Transportation Improvement Program.

BE IT FURTHER RESOLVED that a copy of this resolution is forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

Summary of Action

Minor Amendment to the FY 2022 – 2025 Transportation Improvement Program to Add the Boylan Terrace Neighborhood Pedestrian Connection Project as Requested by the Borough of Bernardsville

Action: In order to advance the following project, the North Jersey Transportation Planning Authority (NJTPA), on behalf of the Borough of Bernardsville, has requested amending the FY 2022 – 2025 Transportation Improvement Program (TIP) to program federal funds.

Background: According to the *TIP Memorandum of Understanding*, when a project or program that uses federal funds is added to the TIP and a new air quality conformity determination is not required, this constitutes a minor amendment, and action is required by the NJTPA Project Prioritization Committee and the Board of Trustees.

This project is exempt from an air quality conformity determination and does not impact the current conformity determination. Funds for this project have been appropriated by the U.S. Senate Transportation, Housing and Urban Development (THUD) subcommittee as a Federal Fiscal Year (FFY) 2022 Community Project Funding/Congressionally Directed Spending grant. The funds are made available through the U.S. Department of Transportation (U.S. DOT) Highway Infrastructure Programs (HIP). The attached documents provide further details on the project.

Minor Amendment:

- **Boylan Terrace Neighborhood Connection, Somerset County DB# N2308**
The Borough of Bernardsville, having received a THUD, U.S. DOT HIP Community Project Funding/Congressionally Directed Spending grant in the amount of \$1.427 million, has requested that this project be added to the TIP. The project will be programmed as follows: Preliminary Engineering in FFY 2023 for \$0.095 million; Final Design in FFY 2024 for \$0.072 million; Right of Way in FFY 2024 for \$0.040 million; and Construction in FFY 2025 for \$1.221 million.

Justification for Action: Approval of this revision to the FY 2022 – 2025 TIP will allow the funds to be secured and the referenced project to advance.

Staff Recommendation: Central Staff recommends approval of this action.

PENDING ACTION

NJTPA

Transportation Improvement Program Fiscal Years 2022 - 2025

Routes: Mileposts: DBNUM: N2308

Name: Boylan Terrace Neighborhood Pedestrian Connection

The Borough of Bernardsville is proposing a pedestrian connection between the Bernardsville Train Station and the Borough's downtown area and a large, primarily residential neighborhood located to the south of the Mine Brook and the New Jersey Transit tracks.

The proposed pedestrian connector will include an at grade sidewalk that will run along the north side of Boylan Terrace from Mount Airy Road to West Street. At the intersection of Boylan Terrace and West Street, the at grade sidewalk will connect to a prefabricated pedestrian bridge structure. The pedestrian bridge will provide a series of ADA compliant ramps that will span the grade change and the Mine Brook. The pedestrian bridge will meet existing grade within the existing parking area to the south of the train tracks at Depot Place. An at grade pedestrian crossing will be required to cross the train tracks and continue to the train station and downtown areas via the existing sidewalk network on Mine Brook Road (Route 202). The following federal appropriation was programmed to this project: DEMO ID# NJ307.

Counties: Somerset

Municipalities: Bernardsville Boro

RCIS Category: Bike/Ped

Sponsor: Borough of Bernardsville

Est. Total Project Cost: (Million) \$1.428

TIP:

PENDING

FY 2022 - 2025 TIP Cost: (Million) \$1.428

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	PE	DEMO		\$.095			
NJTPA	DES	DEMO			\$.072		
NJTPA	ROW	DEMO			\$.040		
NJTPA	CON	DEMO				\$1.221	
				\$.095	\$.112	\$1.221	

DATE SUBMITTED: 1/11/2023



State of New Jersey

DEPARTMENT OF TRANSPORTATION
1035 Parkway Avenue
PO Box 600
Trenton, New Jersey 08625-0600

MPO: **NJTPA**

PROJECT: **Boylan Terrace Neighborhood Pedestrian Connection**

DBNUM: N2308

TRACK #: N-N2308-0-A-2022

TYPE OF PROJECT CHANGE: Project Phase Addition

ACTION TAKEN: Adding Preliminary Engineering in 2023 for \$0.095 million of DEMO funding adding Design in 2024 for \$0.072 million of DEMO funding adding Right Of Way in 2024 for \$0.040 million of DEMO funding and adding Construction in 2025 for \$1.1221 million of DEMO funding.

REASON FOR CHANGE:

This project received funding through the FY2022 Federal Transportation, Housing and Urban Development (THUD) Highway Infrastructure Program (HIP) and is ready to be entered into the TIP.

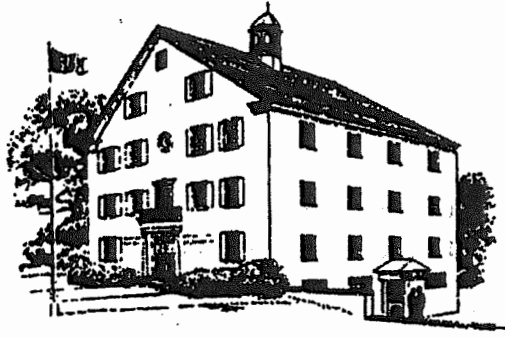
MPO ACTION REQUIRED: Action requires MPO board approval

FISCAL CONSTRAINT:

Comments: THUD HIP FY2022 grant recipient

CONFORMITY:

PUBLIC INVOLVEMENT:



Borough of Bernardsville
166 Mine Brook Road
Bernardsville, NJ 07924
Somerset County

Administration (908) 766-3000 Fax (908) 766-2401
Engineering (908) 766-3850 Fax (908) 766-2788

November 1, 2022

Mr. David W. Behrend
Acting Executive Director
New Jersey Transportation Planning Authority
One Newark Center, 17th Floor
Newark, NJ 07102

Re: FY22 HIP Community Funding Project - Borough of Bernardsville Boylan Terrace Neighborhood
Pedestrian Connection, Borough of Bernardsville, Somerset County

Dear Mr. Behrend,

The Borough of Bernardsville is hereby requesting that the above mentioned project be entered into the TIP Program. The Concept Development Study was completed in 2022. An RFP for Preliminary Engineering will be advertised shortly. The Borough of Bernardsville anticipates the following future costs.

FY	Phase	Estimated Cost
2023	PE	\$95,000.00
2024	FD	\$71,500.00
2024	ROW	\$40,000.00
2024	CON	\$1,221,000.00
Total Estimated Cost		\$1,427,500.00

This project will provide walkable links to other transportation modes as well as provide access to downtown businesses, shopping, restaurants, Farm Market and other amenities to an area of the Borough that contains approximately 70% of the Borough's stand-alone rental properties.

While this new connection will promote connectivity in and around the downtown, the improvement to the pedestrian circulation network will help create new energy and economic activity, in line with community objectives.

Please contact me should you have any questions at 908.766.3000 ext.144

Sincerely,

Douglas Walker, Project Manager

Attachment H.2.

**DRAFT RESOLUTION: MINOR AMENDMENT TO THE FY 2022 – 2025
TRANSPORTATION IMPROVEMENT PROGRAM TO
ADD THE NJTPA CARBON REDUCTION PROGRAM AS
REQUESTED BY THE NORTH JERSEY
TRANSPORTATION PLANNING AUTHORITY**

WHEREAS, the North Jersey Transportation Planning Authority (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the NJTPA formally adopted the FY 2022 – 2025 Transportation Improvement Program (TIP) on September 13, 2021; and

WHEREAS, the NJTPA on September 24, 2012 approved a Memorandum of Understanding (MOU) among the NJTPA, the New Jersey Department of Transportation (NJDOT), and NJ TRANSIT on procedures to amend and modify the State Transportation Improvement Program and the NJTPA TIP; and

WHEREAS, the TIP may be revised any time; and

WHEREAS, according to the MOU when a project or program that uses federal funds is added to the TIP and a new air quality conformity determination is not required, this constitutes a minor amendment; and

WHEREAS, the NJTPA has requested the addition of the NJTPA Carbon Reduction Program (DB# N2309) to the FY 2022 – 2025 TIP; and

WHEREAS, the Infrastructure Investment and Jobs Act provides the resources for this program; and

WHEREAS, this minor amendment is exempt from an air quality conformity determination and does not impact the current conformity determination; and

WHEREAS, Congestion Management Process requirements do not apply to these actions; and

WHEREAS, the FY 2022 – 2025 TIP conforms to the performance-based planning requirements of the Infrastructure Investment and Jobs Act and its predecessor legislation; and

WHEREAS, the NJTPA has provided opportunities for review of these actions consistent with NJTPA public participation procedures; and

WHEREAS, no action authorized by the NJTPA shall have force or effect until 10 days (Saturdays, Sundays and public holidays excepted) after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of

the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the specified amendment to the FY 2022 – 2025 Transportation Improvement Program.

BE IT FURTHER RESOLVED that a copy of this resolution is forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

Summary of Action

Minor Amendment to the FY 2022 – 2025 Transportation Improvement Program to Add the NJTPA Carbon Reduction Program as Requested by the North Jersey Transportation Planning Authority

Action: In order to advance local projects that support greenhouse gas emission reductions, the North Jersey Transportation Planning Authority (NJTPA) has requested amending the FY 2022 – 2025 Transportation Improvement Program (TIP) to program federal funds for a regional Carbon Reduction Program.

Background: According to the *TIP Memorandum of Understanding*, when a new project or program that uses federal funds is added to the TIP and a new air quality conformity determination is not required, this constitutes a minor amendment, and action is required by the NJTPA Project Prioritization Committee and the Board of Trustees.

Program line items are exempt from air quality conformity determination. Once projects are identified for funding through this program, they will be classified for air quality conformity. Funds are available for this program from the Infrastructure and Investment Jobs Act (IIJA) for use by Metropolitan Planning Organizations such as the NJTPA. The attached documents provide further details on the program.

Minor Amendment:

- **NJTPA Carbon Reduction Program, DB# N2309**

This new program will provide funds from the IIJA for local level projects in the NJTPA region that support greenhouse gas emission reductions for the transportation system. Suballocated urbanized funds in the amount of \$56.113 million have been specified for this purpose from Federal Fiscal Years 2023 through 2025 for Engineering, Right of Way, and Construction.

Justification for Action: Approval of this revision to the FY 2022 – 2025 TIP will allow the funds to be secured and the referenced program to advance.

Staff Recommendation: Central Staff recommends approval of this action.

PENDING ACTION

NJTPA

Transportation Improvement Program Fiscal Years 2022 - 2025

Routes: Mileposts: DBNUM: N2309

Name: NJTPA Carbon Reduction Program

This program provides funds for projects in the NJTPA metropolitan region that support greenhouse gas emission reductions from the transportation system. Projects may include but are not limited to; planning, design, and construction of public transportation projects and improvements, community transportation and shared micro-mobility projects, transportation alternatives (including construction, planning and design of on and off-road trail facilities), the deployment of electric vehicles, and other activities that reduce carbon dioxide and other greenhouse gas emissions in the region.

Counties: Various **Municipalities:** Various

RCIS Category: Environment/Air Quality

Sponsor: NJTPA **Est. Total Project Cost: (Million) \$149.070**

TIP:

PENDING

FY 2022 - 2025 TIP Cost: (Million) \$56.113

Unconstrained Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	ERC	CR-ALLEN		\$0.144	\$0.074	\$0.076	\$0.486
NJTPA	ERC	CR-NY/NWK		\$27.307	\$14.064	\$14.346	\$92.303
NJTPA	ERC	CR-PGH/NWB		\$0.050	\$0.026	\$0.026	\$0.168
				\$27.501	\$14.164	\$14.448	\$92.957



State of New Jersey

DEPARTMENT OF TRANSPORTATION
1035 Parkway Avenue
PO Box 600
Trenton, New Jersey 08625-0600

MPO: **NJTPA**

PROJECT: **NJTPA Carbon Reduction Program**

DBNUM: N2309

TRACK #: N-N2309-0-A-2022

TYPE OF PROJECT CHANGE:

ACTION TAKEN: Adding CR-ALLEN funds for ERC in FFY2023 in the amount of \$0.144, in FFY 2024 in the amount of \$0.074 in FFY 2025 in the amount of \$0.076 in FFY 2026 in the amount of \$0.077 in FFY 2027 in the amount of \$0.079 in FFY 2028 in the amount of \$0.080 in FFY 2029 in the amount of \$0.082 in FFY 2030 in the amount of \$0.083 in FFY 2031 in the amount of \$0.085.

Adding CR-NY/NWK for ERC in FFY 2023 in the amount of \$27.306, in FFY 2024 in the amount of \$14.064, in FFY 2025 in the amount of \$14.346, in FFY 2026 in the amount of \$14.632, in FFY 2027 in the amount of \$14.925, in FFY 2028 in the amount of \$15.223, in FFY 2029 in the amount of \$15.528, in FFY 2030 in the amount of \$15.839, in FFY 2031 in the amount of \$16.155.

Adding CR-PGH/NWB for ERC in FFY 2023 in the amount of \$0.50, in FFY 2024 in the amount of \$0.026, in FFY 2025 in the amount of \$0.026, in FFY 2027 in the amount of \$0.027, in FFY 2028 in the amount of \$0.028, in FFY 2029 in the amount of \$0.028, in FFY 2030 in the amount of \$0.029, in FFY 2031 in the amount of \$0.029.

REASON FOR CHANGE:

Suballocated urbanized funds in the amount of \$56.113 million have been specified for this purpose from FFYs 2023 through 2025 for Engineering, Right of Way, and Construction (ERC).

MPO ACTION REQUIRED: Action requires MPO board approval

FISCAL CONSTRAINT:

Comments: suballocated urbanized CR funds

CONFORMITY:

PUBLIC INVOLVEMENT:

Attachment H.3.

**DRAFT RESOLUTION: MINOR AMENDMENTS TO THE FY 2022 – 2025
TRANSPORTATION IMPROVEMENT PROGRAM TO
ADD FEDERAL FUNDS TO THREE PROJECTS AS
REQUESTED BY THE NEW JERSEY DEPARTMENT OF
TRANSPORTATION**

WHEREAS, the North Jersey Transportation Planning Authority (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the NJTPA formally adopted the FY 2022 – 2025 Transportation Improvement Program (TIP) on September 13, 2021; and

WHEREAS, the NJTPA on September 24, 2012 approved a Memorandum of Understanding (MOU) among the NJTPA, the New Jersey Department of Transportation (NJDOT), and NJ TRANSIT on procedures to amend and modify the State Transportation Improvement Program and the NJTPA TIP; and

WHEREAS, the TIP may be revised any time; and

WHEREAS, according to the MOU, when federal funds above the specified threshold are added to a project or program in the TIP, and a new air quality conformity determination is not required, this constitutes a minor amendment; and

WHEREAS, the NJDOT has requested the addition of federal funds to the Portway, Fish House Road/Pennsylvania Avenue, CR 659 project in Hudson County (DB# 97005B), to the Route 7, Kearny, Drainage Improvements project in Hudson County (DB# 93186), and to the Route 23, Bridge over Pequannock River/Hamburg Turnpike project in Morris and Passaic Counties (DB# 08347); and

WHEREAS, funds are available for these purposes; and

WHEREAS, these minor amendments are exempt from an air quality conformity determination and do not impact the current conformity determination; and

WHEREAS, Congestion Management Process requirements do not apply to these actions; and

WHEREAS, the FY 2022 – 2025 TIP conforms to the performance-based planning requirements of the Infrastructure Investment and Jobs Act and its predecessor legislation; and

WHEREAS, the NJTPA has provided opportunities for review of these actions consistent with NJTPA public participation procedures; and

WHEREAS, no action authorized by the NJTPA shall have force or effect until 10 days (Saturdays, Sundays and public holidays excepted) after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the specified amendment to the FY 2022 – 2025 Transportation Improvement Program.

BE IT FURTHER RESOLVED that a copy of this resolution is forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

Summary of Action

Minor Amendments to the FY 2022 – 2025 Transportation Improvement Program to Add Federal Funds to Three Projects as Requested by the New Jersey Department of Transportation

Action: In order to advance the following projects, the New Jersey Department of Transportation (NJDOT) has requested amending the FY 2022 – 2025 Transportation Improvement Program (TIP) by updating programmed funding for them as detailed below.

Background: According to the *TIP Memorandum of Understanding*, when funds are added above the specified threshold and a new air quality conformity determination is not required, this constitutes a minor amendment, and action is required by the NJTPA Project Prioritization Committee and the Board of Trustees. The requested funding revisions detailed below are within that threshold.

The revisions have been reviewed by Capital Programming staff and the counties of Hudson, Morris, and Passaic. Fiscal constraint is maintained for these changes, through funds available from prior year unobligated balances, the New Jersey State Transportation Trust Fund, and the Infrastructure for Rebuilding America (INFRA) discretionary grant program. These projects are exempt from an air quality conformity determination and do not impact the current conformity determination. Additional project and funding information is provided in the attached documents.

Minor Amendments:

- **Portway, Fish House Road/Pennsylvania Avenue, CR 659, *Hudson County*
DB# 97005B**
This request is for the addition of Surface Transportation Block Grant Program (STBGP) funds in the amount of \$21.531 million, for a revised total project amount of \$65.931 million for construction in Federal Fiscal Year (FFY) 2023. The increase is necessary due to the updated cost estimate.
- **Route 7, Kearny, Drainage Improvements, *Hudson County* DB# 93186**
This request is for the addition and advancement of National Highway Performance Program (NHPP) funds for construction in FFY 2023 for a revised total of \$82.027 million applied to the project. In addition, INFRA grant funds will be added in FFY 2023 in the amount of \$26 million and State funds will be added in the amount of \$7 million. This will result in a revised total of \$115.027 million for construction in FFY 2023. The increase is necessary due to recent increases in material costs, particularly for steel sheeting, iron pipes, pump station equipment, and utilities.
- **Route 23, Bridge over Pequannock River/Hamburg Turnpike, *Morris and Passaic Counties* DB# 08347**
This request is for the addition of \$30.589 million of Bridge Formula Program (BFP) funds, for a revised total of \$90.700 million for construction in FFY 2023. The increase is

necessary due to changes in the project's design and increased costs for structural steel, concrete and mobilization.

Justification for Action: Approval of these revisions to the FY 2022 – 2025 TIP will allow the funds to be secured and the referenced projects to advance.

Staff Recommendation: Central Staff recommends approval of these actions.

PENDING ACTION

NJTPA

Transportation Improvement Program Fiscal Years 2022 - 2025

Routes: / CR 659

Mileposts: 0.5-1.4

DBNUM: 97005B

Name: Portway, Fish House Road/Pennsylvania Avenue, CR 659

This project provides roadway reconstruction. The project includes two 12-ft lanes, and a 12-ft shoulder, Eastbound and Westbound, along Pennsylvania Avenue/Fish House Road. Sidewalks will be provided along the Eastbound side of Central Avenue.

Counties: Hudson

Municipalities: Kearny Town

RCIS Category: Freight

Sponsor: NJDOT

Est. Total Project Cost: (Million) \$75.033

TIP:

CURRENT

FY 2022 - 2025 TIP Cost: (Million) \$44.400

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	CON	NHFP-HWY		\$24.876			
NJTPA	CON	STBGP-FLEX		\$19.524			
				\$44.400			

PENDING

FY 2022 - 2025 TIP Cost: (Million) \$65.931

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	CON	NHFP-HWY		\$24.876			
NJTPA	CON	STBGP-FLEX		\$41.055			
				\$65.931			

DATE SUBMITTED: 1/24/2023



State of New Jersey

DEPARTMENT OF TRANSPORTATION
1035 Parkway Avenue
PO Box 600
Trenton, New Jersey 08625-0600

MPO: **NJTPA**

PROJECT: **Portway, Fish House Road/Pennsylvania Avenue, CR 659**

DBNUM: 97005B

TRACK #: N-97005B-2-M-2022

TYPE OF PROJECT CHANGE: Project Cost increase

ACTION TAKEN: Increases STBGP-FLEX funding for CON in FFY 23 from \$19.524M by \$21.531M to \$41.055M
Increases total programmed CON from \$44.4M by \$21.531M to \$65.931M

REASON FOR CHANGE:

MPO ACTION REQUIRED: Action requires MPO board approval

FISCAL CONSTRAINT:

Comments: See NJTPA FY 22-31 Chart 65

CONFORMITY:

PUBLIC INVOLVEMENT:

PENDING ACTION

NJTPA

Transportation Improvement Program Fiscal Years 2022 - 2025

Routes: 7 /

Mileposts: 1.5 - 3.8

DBNUM: 93186

Name: Route 7, Kearny, Drainage Improvements

This section of Route 7 is generally uncurbed and frequently flooded due to low elevation and lack of sufficient highway drainage system. Roadway runoff is collected through inlets or sheet flow, discharging directly into the marshlands. During moderate and heavy storms, in addition to high tide, the runoff overflows the banks onto the roadway and adjacent properties. This causes the highway to be closed and traffic is detoured. This project will provide highway drainage system improvements including; pumping stations, raising road profile and sheet piling to prevent tidal water to flood the roadway.

Counties: Hudson

Municipalities: Kearny Town

RCIS Category: Road Preservation

Sponsor: NJDOT

Est. Total Project Cost: (Million) \$128.995

TIP:

CURRENT

FY 2022 - 2025 TIP Cost: (Million) \$82.700

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	CON	NHPP		\$25.000	\$25.000	\$32.700	
				\$25.000	\$25.000	\$32.700	

PENDING

FY 2022 - 2025 TIP Cost: (Million) \$115.027

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	CON	INFRA		\$26.000			
NJTPA	CON	NHPP		\$82.027			
NJTPA	CON	STATE		\$7.000			
				\$115.027			



State of New Jersey

DEPARTMENT OF TRANSPORTATION
1035 Parkway Avenue
PO Box 600
Trenton, New Jersey 08625-0600

MPO: **NJTPA**

PROJECT: **Route 7, Kearny, Drainage Improvements**

DBNUM: 93186

TRACK #: N-93186-2-M-2022

TYPE OF PROJECT CHANGE: Project Cost increase

ACTION TAKEN: Advances multi-year NHPP funding for CON in the amount of \$25M from FFY 24 to FFY 23
Advances multi-year NHPP funding for CON in the amount of \$32.7M from FFY 25 to FFY 23
Reduces NHPP funding for CON in FFY 23 from \$82.7M by \$.673M to \$82.027M
Adds INFRA grant of \$26M in FY 23 for CON
Adds \$7M of STATE in FY 23 for CON
Total CON increase of \$32.327M for a total programmed amount of \$115.027M
Updates the project Mileposts from 1.7 - 3.8 to 1.5 - 3.8

REASON FOR CHANGE:

Per the PM of the project: The cost was increased almost 34% due the cost increase of materials especially steel after COVID 19, due to inflation and material availability issues. That include steel sheeting, Ductile Iron pipes, Pump stations equipment and other items including utility cost.

MPO ACTION REQUIRED: Action requires MPO board approval

FISCAL CONSTRAINT:

Comments: See NJTPA FY 22-31 Chart 65

CONFORMITY:

PUBLIC INVOLVEMENT:

PENDING ACTION

NJTPA

Transportation Improvement Program Fiscal Years 2022 - 2025

Routes: 23 /

Mileposts: 16.61 - 17.34

DBNUM: 08347

Name: Route 23, Bridge over Pequannock River / Hamburg Turnpike

Initiated by the Bridge Management System, this project will replace the bridge, built in 1934, and provide scour countermeasures to address this scour critical structure.

Counties: Morris Passaic

Municipalities: Kinnelon Boro West Milford Twp

RCIS Category: Bridges

Sponsor: NJDOT

Est. Total Project Cost: (Million) \$90.700

TIP:

CURRENT

FY 2022 - 2025 TIP Cost: (Million) \$60.111

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	CON	BFP		\$60.111			
				\$60.111			

PENDING

FY 2022 - 2025 TIP Cost: (Million) \$90.700

Unconstrained
Information Year

MPO	PHASE	SOURCE	2022	2023	2024	2025	2026-2031
NJTPA	CON	BFP		\$90.700			
				\$90.700			



State of New Jersey

DEPARTMENT OF TRANSPORTATION
1035 Parkway Avenue
PO Box 600
Trenton, New Jersey 08625-0600

MPO: **NJTPA**

PROJECT: **Route 23, Bridge over Pequannock River / Hamburg Turnpike**

DBNUM: 08347

TRACK #: N-08347-2-M-2022

TYPE OF PROJECT CHANGE: Project Cost increase

ACTION TAKEN: Increases BFP funding for CON in FFY 23 from \$60.111M by \$30.589M to \$90.700M.

REASON FOR CHANGE:

Per the PM of the project, the following is the justification for increase in the construction cost estimate:

- Change in the substructure foundation type from drilled shafts to micropiles due to constructability and construction access issues.
- The change in skew lengthened the substructure by 20%, and subsequently increased the amount of columns at each pier from 4 to 5.
- The adjusted span arrangement based on increased skew and substructure lengthening increased the overall length of the bridge.
- Additional riverbank stabilization by using A-Jacks has been added to the project.
- Significant increased prices for structural steel and concrete.
- The mobilization cost has also increased.

MPO ACTION REQUIRED: Action requires MPO board approval

FISCAL CONSTRAINT:

Comments: See NJTPA FY 22-31 Chart 65

CONFORMITY:

PUBLIC INVOLVEMENT:

Attachment H.4.

DRAFT RESOLUTION: APPROVAL OF PROJECTS FOR THE FY 2022 LOCAL SAFETY PROGRAM AND HIGH RISK RURAL ROADS PROGRAM

WHEREAS, the North Jersey Transportation Planning Authority (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the Local Safety Program and High Risk Rural Roads Program are supported with funds from the Federal Highway Administration's Highway Safety Improvements Program (HSIP); and

WHEREAS, the Local Safety Program (LSP) provides an opportunity for eligible applicants to apply for federal funding for the construction of safety improvements on county and eligible local roadway facilities within the region which have crash rates that exceed the NJTPA region's average for those functional classes of roadways; and

WHEREAS, High Risk Rural Roads Program (HRRRP) provides an opportunity for eligible applicants to apply for federal funding for the construction of safety improvements on roadways that are functionally classified as a rural major or minor collector or as a local rural road and have crash rates that exceed the NJTPA region's average for those functional classes of roadways; and

WHEREAS, the NJTPA sent out solicitation notifications in December 2021 inviting eligible applicants to submit for the programs; and

WHEREAS, NJTPA received fifteen (15) eligible Local Safety Program applications and three High Risk Rural Roads Program applications in the NJTPA region; and

WHEREAS, a Technical Review Committee composed of staff from NJTPA and NJDOT reviewed the applications and recommends the inclusion of all projects in the FY 2022 Local Safety and High Risk Rural Roads Programs with funding of \$160.3 million and \$20.2 million respectively; and

WHEREAS, the projects requesting design assistance will be included in the FY 2024 Local Safety Engineering Assistance Program (LSEAP); and

WHEREAS, the JFK (Kennedy) Boulevard from Pavonia Avenue to St. Paul's Avenue project has concluded the Local Concept Development Phase and has been recommended by an Interagency Review Committee to advance to the Preliminary Engineering phase using HSIP funding and will be included in the FY 2024 LSEAP; and

WHEREAS, the recommended projects address the goals of the programs; and

WHEREAS, no action authorized by the NJTPA shall have force or effect until ten (10) days, Saturdays, Sundays and public holidays excepted, after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the attached list of projects for the Local Safety Program and the High Risk Rural Roads Program.

BE IT FURTHER RESOLVED, that a copy of this resolution be forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration and to the counties of the selected applicants.

Summary of Action

Approval of Projects for the FY 2022 Local Safety Program and High Risk Rural Roads Program

Action: Approval of projects for the Federal Fiscal Year 2022 Local Safety Program (LSP) and High Risk Rural Roads Program (HRRRP).

Background: The North Jersey Transportation Planning Authority began the solicitation of projects for the Fiscal Year 2022 LSP and HRRR on December 21, 2021. Both programs use funds from the Federal Highway Administration's Highway Safety Improvements Program (HSIP). Applications for the programs are reviewed by a Technical Review Committee (TRC) composed of NJTPA Central Staff, staff from the New Jersey Department of Transportation (NJDOT) Division of Local Aid, Division of Safety, Bicycle and Pedestrian Programs and Bureau of Environmental Program Resources; and staff from the Federal Highway Administration (FHWA).

The LSP was established by the NJTPA, in conjunction with NJDOT in 2005, in order to advance safety improvements on county and eligible local roadways within the region. Since its inception, over \$340 million in projects have been funded through the program. Projects have included modern roundabouts, road diets, new and upgraded traffic signals, curb extensions, median refuge islands, and other improvements to increase the safety of drivers, bicyclists and pedestrians.

The HRRRP provides the region with funds to advance safety improvements on rural roadways that have been identified as high risk. These roadways are functionally classified as a rural major or minor collector or as a local rural road and have crash rates that exceed the NJTPA region's average for those functional classes of roadways. Since its inception in 2009, over \$60 million in projects have been funded through the program.

Description of Action: The TRC is recommending approval of 15 Local Safety projects (total \$160.3 million) and three High Risk Rural Roads projects (total \$20.2 million). Projects requesting design assistance will be included in the FY 2024 Local Safety Engineering Assistance Program (LSEAP).

An additional project, the JFK (Kennedy) Boulevard from Pavonia Avenue to St. Paul's Avenue project, has completed a Local Concept Development study and has been recommended by an Interagency Review Committee (IRC) for \$7.8 million in HSIP funding. The IRC is composed of NJTPA Central Staff, and staff from the NJDOT Division of Local Aid, the NJDOT Bureau of Environmental Program Resources, and the Federal Highway Administration. This project will also be included in the FY 2024 LSEAP. An attached spreadsheet provides further project details.

Justification for Action: Approval of this action will allow the funds to be secured and the referenced projects to advance.

Staff Recommendation: Central Staff recommends approval of this action.

FY 2022 LOCAL SAFETY AND HIGH RISK RURAL ROADS PROGRAMS APPLICATIONS

Sponsor	Municipality	Project Name and Location	Milepost Limits	Project Description	Design Assistance	Construction Inspection	Estimated Construction Cost	Estimated Design Cost	Estimated Construction Inspection	Total Cost
1	Essex County	Bloomfield, Glen Ridge & Newark Bloomfield Avenue and Ridgewood Avenue Safety Improvement Project	Bloomfield Avenue CR 506S - MP 0.00-3.93 Ridgewood Avenue CR 653 - MP 1.21-2.07	The purpose of the project is to enhance the safety and mobility along both Project Routes: Bloomfield Avenue (C.R. 506S) and Ridgewood Avenue (C.R 653).	No	Yes	\$ 19,234,163	\$ -	\$ 2,500,441	\$ 21,734,604
2	Essex County	East Orange, Irvington, South Orange, Maplewood & Newark Grove Street and Irvington Avenue / Clinton Avenue Safety Improvement Project	Grove Street (N/S) CR 509 - MP 14.00-23.00 Irvington Ave / Clinton Ave CR 665 - MP 0.00-2.22	The purpose of the project is to enhance the safety and mobility along both Project Routes: Bloomfield Avenue (C.R. 506S) and Ridgewood Avenue (C.R 653).	No	Yes	\$ 21,973,420	\$ -	\$ 2,856,545	\$ 24,829,965
3	Hudson County	North Bergen, Hoboken, Jersey City & Union City Safety Improvements to JFK Boulevard East (Main Drive to Palisades Triangle Plaza) and Paterson Plank Road (Harrison St. to S. Wing Viaduct)	JFK Blvd East CR 693 - MP 1.94-2.06 Paterson Plank Road CR 681 - MP 0.71-1.64	The overall purpose of this project is to enhance safety along Boulevard East between Main Drive and Palisades Triangle Plaza in North Bergen Township and along Paterson Plank Road between Harrison Street and S. Wing Viaduct in Hoboken, Jersey City and Union City.	Yes	Yes	\$ 8,713,163	\$ 2,178,291	\$ 1,132,711	\$ 12,024,165
4	Hudson County	Jersey City Improvements to JFK Blvd - From Route 139 to Secaucus Road	JFK Blvd CR 501 - MP 31.10-32.60	The purpose of the project is to enhance safety and mobility for all modes along Kennedy Blvd Between Route 139 and Secaucus Road in Jersey City. A total of 699 reportable crashes during the three-year analysis period of 2017–2019 along this 1.5 mile segment	Yes	Yes	\$ 18,412,466	\$ 4,603,117	\$ 2,393,621	\$ 25,409,203
5	Jersey City	Jersey City Summit Avenue Safety Improvements	09061571 - MP 0.00-0.53, 09061700 - MP 0.00-1.03	Project is to enhance safety along Summit Avenue between Route 139/St. Paul's Avenue and 5th Street/Secaucus Road in Jersey City. A total of 217 crashes occurred 2017–2019 within the approximately 1.6 mile segment = 17.19 (crashes/MVM).	Yes	Yes	\$ 5,899,630	\$ 1,474,908	\$ 766,952	\$ 8,141,489
6	Middlesex County	Perth Amboy Smith Street Corridor Improvements	MP 2.81 to MP 4.21	The project is to improve safety and mobility for all road users along Smith Street between Convery Boulevard and Front Street in the City of Perth Amboy.	Yes	Yes	\$ 7,542,000	\$ 1,885,500	\$ 980,460	\$ 10,407,960
7	Middlesex County	Piscataway Stelton Road Corridor Improvements	12000665 - MP 1.45-2.60 00000529 - MP 5.20-5.31	This project aims to improve bicycle and pedestrian safety along the Stelton Road Corridor within the project area. This segment of Stelton Road ranks eighth in NJTPA's network screening list Middlesex County's Pedestrian and Bicycle corridor list.	Yes	Yes	\$ 5,623,500	\$ 1,405,875	\$ 731,055	\$ 7,760,430
8	Monmouth County	Millstone & Jackson Monmouth Road	MP 41.83 to 42.37	The purpose of the project is to enhance safety and mobility along CR 537(Monmouth Rd) between Burnt Tavern/Anderson Rd. and Allyson Rd. in Millstone and Jackson Townships.	Yes	Yes	\$ 2,143,830	\$ 535,958	\$ 278,698	\$ 2,958,485
9	Monmouth County	Long Branch Cedar Avenue	MP 0.09 to 0.99	The purpose of the project is to enhance safety and mobility along CR 57 (Cedar Ave) between CR 25 (Norwood Ave) and CR 57 (Ocean Ave) in Long Branch.	Yes	Yes	\$ 3,405,804	\$ 851,451	\$ 442,755	\$ 4,700,010
10	Morris County	Mendham Borough & Mendham Township CR 510 Corridor Improvement Project	MP 5.52 to 5.83	It is the goal of this project to improve pedestrian and vehicular safety while minimizing environmental, quality of life, right of way, and utility impacts	Yes	Yes	\$ 1,344,255	\$ 336,064	\$ 174,753	\$ 1,855,072
11	Morris County	Dover South Morris Street (CR 643) Intersection Improvements Project	MP 0.54 to 0.82	The purpose of the project is to enhance safety, improve pedestrian features, reduce crashes, and improve mobility throughout the project limits.	yes	Yes	\$ 851,565	\$ 212,891	\$ 110,703	\$ 1,175,160

FY 2022 LOCAL SAFETY AND HIGH RISK RURAL ROADS PROGRAMS APPLICATIONS

Sponsor	Municipality	Project Name and Location	Milepost Limits	Project Description	Design Assistance	Construction Inspection	Estimated Construction Cost	Estimated Design Cost	Estimated Construction Inspection	Total Cost	
12	Passaic County	Wayne	Valley Road Safety Improvements	MP 0.0 to 3.22	Valley Road in Wayne Township from French Hill Road (MP0.0) to Hamburg Turnpike(MP3.22) runs north-south and the civic center of Wayne is located on the corridor. The civic center included the township municipal complex, library, and high school.	Yes	Yes	\$ 10,900,000	\$ 2,725,000	\$ 1,417,000	\$ 15,042,000
13	Passaic County	Clifton	Lakeview Ave Clifton Safety Improvements	MP 1.02 to 2.16	The purpose of the project is to enhance safety and mobility along Lakeview Ave in the City of Clifton between Clifton Ave and Crooks Ave.	Yes	Yes	\$ 4,260,000	\$ 1,065,000	\$ 553,800	\$ 5,878,800
14	Somerset County	Bound Brook	County Routes 533 & 527 Tea Street to Bound Brook Border	CR533 - MP 31.42-32.78 & CR527 - MP 53.27-53.74	The purpose of the project is to enhance safety and mobility along Talmage Avenue, Columbus Place and Main Street from Tea Street to the municipal boundary of Bound Brook.	Yes	Yes	\$ 5,579,000	\$ 1,394,750	\$ 725,270	\$ 7,699,020
15	Union County	Various	Intersection Improvements at Various Intersections	Various	The purpose of the project is to enhance safety and mobility of the traveling public along various high traffic areas within Union County. The traveling public includes those in vehicles and those walking.	Yes	Yes	\$ 7,760,000	\$ 1,940,000	\$ 1,008,800	\$ 10,708,800

\$ 123,642,796	\$ 20,608,803	\$ 16,073,563	\$ 160,325,162
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FY 2022 LOCAL SAFETY AND HIGH RISK RURAL ROADS PROGRAMS APPLICATIONS

Sponsor	Municipality	Project Name and Location	Milepost Limits	Project Description	Design Assistance	Construction Inspection	Estimated Construction Cost	Estimated Design Cost	Estimated Construction Inspection	Total Cost
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Local Concept Development to Local Safety Program

16	Hudson County	Jersey City	Improvements to JFK Blvd - From Pavonia Avenue to St. Paul's Avenue	JFK Blvd CR 501 - MP 30.78-31.10	The purpose of the project is to enhance safety and mobility along with reducing congestion along JFK (Kennedy) Boulevard between Pavonia Avenue and St Paul's Avenue.	Yes	Yes	\$ 5,655,765	\$ 1,413,941	\$ 735,249	\$ 7,804,956
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\$ 5,655,765	\$ 1,413,941	\$ 735,249	\$ 7,804,956
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HRRR

17	Monmouth County	Upper Freehold & Millstone	CR 526 - Trenton - Lakewood Road	MP 12.82 to 18.18	The purpose of this project is to reduce crash occurrence and improve traffic safety along the CR 526 corridor within the Townships of Upper Freehold and Millstone.	Yes	Yes	\$ 4,310,710	\$ 1,077,678	\$ 560,392	\$ 5,948,780 *
18	Monmouth County	Manalapan & Englishtown	CR 527A - Iron Ore Road / High Bridge Road	MP 2.87 to 5.79	The purpose of this project is to reduce crash occurrence and improve traffic safety along the CR 527A corridor within the Township of Manalapan.	Yes	Yes	\$ 3,467,285	\$ 866,821	\$ 450,747	\$ 4,784,853
19	Monmouth County	Upper Freehold	CR 539 - Allentown-Davis Station Road / Forked River Road	MP 37.71 to 43.72	The purpose of this project is to reduce crash occurrence and improve traffic safety along the CR 539 corridor within the Township of Upper Freehold.	Yes	Yes	\$ 6,847,360	\$ 1,711,840	\$ 890,157	\$ 9,449,357

\$ 14,625,355	\$ 3,656,339	\$ 1,901,296	\$ 20,182,990
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\$ 143,923,916	\$ 25,679,083	\$ 18,710,109	\$ 188,313,108
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Attachment H.5.

**DRAFT RESOLUTION: APPROVAL OF THE INITIAL FINANCIAL PLAN
FOR THE ROUTE 7, KEARNY DRAINAGE
IMPROVEMENTS PROJECT**

WHEREAS, the North Jersey Transportation Planning Authority (NJTPA) is the Metropolitan Planning Organization (MPO) responsible for updating the Transportation Improvement Program (TIP) for the northern part of New Jersey as required by 23 CFR 450 and CFR 613 B; and

WHEREAS, under federal law, recipients of federal financial assistance for projects with a total cost between \$100 million and \$500 million are required to prepare a financial plan; and

WHEREAS, a Financial Plan addresses the following elements:

- **Project Description:** A detailed description that presents the scope of the project. A graphic depicting the project location is recommended.
- **Cost Estimate:** The total cost and cost-to-complete for major project elements are presented in year of expenditure dollars.
- **Implementation Plan:** The project schedule is presented and the cost-to-complete is presented in annual increments in year of expenditure dollars.
- **Financing and Revenues:** Presented by funding source as annual elements available for project obligations.
- **Cash Flow:** An annualized presentation of cash income and outgo to illustrate how periodic bills will be paid.
- **Risk Identification and Mitigation Factors:** Identification of the likelihood of issues affecting project completion and sufficiency of revenues – such as cost escalation, construction schedules, and dependencies on future legislation – and strategies and actions to address these risks.
- **Phasing Plan:** Identification of fundable improvements that will address the short-term purpose and need of the project in the event there are insufficient financial resources to complete the entire project.
- **Public Private Partnership (P3):** Assessment of appropriateness of a public-private partnership to deliver the project.
- **Annual Update Cycle:** The submission dates and reporting periods should be proposed in the Initial Financial Plan and annual updated plans; and

WHEREAS, in addition to the financial plan, annual updates are required in order to provide information on actual cost, expenditure and revenue performance; and

WHEREAS, the Route 7, Kearny Drainage Improvements project (DB# 93186) involves raising the roadway profile of Route 7 by approximately 3.5 feet; installation of steel sheeting, three pump stations, and additional inlets; modification of the approach slabs on the Route 7 Bridge over NJ TRANSIT; and electrical modifications to the Route 7 and Porete Avenue signalized intersection; and

WHEREAS, the New Jersey Department of Transportation (NJDOT) has submitted an Initial Financial Plan for the Route 7, Kearny Drainage Improvements, with total costs of approximately \$128.977 million; and

WHEREAS, NJTPA Central Staff has reviewed the Initial Financial Plan and has found the elements listed above to be consistent with federal guidance; and

WHEREAS, NJTPA Central Staff has reviewed the Initial Financial Plan and has found the project to be consistent with the fiscal constraint requirements of the Long Range Transportation Plan and the FY 2022 – FY 2025 TIP as revised; and

WHEREAS, no action authorized by the NJTPA shall have force or effect until ten (10) days, Saturdays, Sundays and public holidays excepted, after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the Initial Financial Plan for the Route 7, Kearny Drainage Improvements Project.

BE IT FURTHER RESOLVED, that a copy of this resolution and attachments be forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration.

Summary of Action

Approval of the Initial Financial Plan for the Route 7, Kearny Drainage Improvements Project

Action: The New Jersey Department of Transportation (NJDOT) is requesting approval of the proposed Initial Financial Plan for the Route 7, Kearny Drainage Improvements project (DB# 93186). The initial financial plan reflects a total project cost of \$128.977 million. The NJTPA Project Prioritization Committee and the NJTPA Board reviews and approves initial financial plans.

Background:

Financial Plans

Under federal law, recipients of federal financial assistance for projects with a total cost from \$100 million to \$500 million are required to prepare a financial plan that includes the following elements:

- **Project Description:** A detailed description that presents the scope of the project. A graphic depicting the project location is recommended.
- **Cost Estimate:** The total cost and cost-to-complete for major project elements are presented in year of expenditure dollars.
- **Implementation Plan:** The project schedule is presented and the cost-to-complete is presented in annual increments in year of expenditure dollars.
- **Financing and Revenues:** Presented by funding source as annual elements available for project obligations.
- **Cash Flow:** An annualized presentation of cash income and outgo to illustrate how periodic bills will be paid.
- **Risk Identification and Mitigation Factors:** Identification of the likelihood of issues affecting project completion and sufficiency of revenues – such as cost escalation, construction schedules, and dependencies on future legislation – and strategies and actions to address these risks.
- **Phasing Plan:** Identification of fundable improvements that will address the short-term purpose and need of the project in the event there are insufficient financial resources to complete the entire project.
- **Public Private Partnership (P3):** Assessment of appropriateness of a public-private partnership to deliver the project.
- **Annual Update Cycle:** The submission dates and reporting periods should be proposed in the Initial Financial Plan and annual updated plans.

The Route 7, Kearny Drainage Improvements Project Initial Financial Plan must be updated annually. The annual updates should identify and resolve any cost and funding changes that have

occurred since the previous submission. This would include changes in project scope that impact the cost estimate and completion schedule of the project.

Project Description

The purpose of this project is to address existing drainage issues along Route 7 between Mileposts 1.70 and 3.80, within the Town of Kearny in Hudson County and abutting the Borough of North Arlington in Bergen County. Route 7 is subject to frequent flooding, which leads to roadway closures, compromises safety for road users, disrupts traffic flow, and contributes to pavement deterioration.

The flooding along Route 7 is mainly due to a lack of adequate roadway drainage systems along the corridor, tidal conditions of the Hackensack River, and the impact of the tidal marshland located adjacent to the roadway. The Route 7, Kearny Drainage Improvements project includes raising the roadway profile of Route 7 by approximately 3.5 feet; installation of steel sheeting, three pump stations, and additional inlets; modification of the approach slabs on the Route 7 Bridge over NJ TRANSIT; and electrical modifications to the Route 7 and Porete Avenue signalized intersection.

The estimated cost of this project is \$128.977 million. As of September 30, 2022, the project has expended \$9.926 million or 7.7 percent of the current project estimate. Construction is anticipated to begin in October 2023 and be completed in July 2029.

Justification for Action: The Initial Financial Plan has been reviewed for consistency based on the elements listed above. The project is identified in the Long Range Transportation Plan and the FY 2022 – FY 2025 Transportation Improvement Program (TIP). The NJTPA Board, at its March meeting, will approve a TIP Minor Amendment to add \$32.327 million of federal funds (including an Infrastructure for Rebuilding America (INFRA) grant of \$26 million) to the Construction phase of work, to cover the remaining anticipated material costs reflected in the initial financial plan.

By approving the Initial Financial Plan, the project will be in compliance with federal guidelines, allowing NJDOT to request authorization for the construction phase of this project.

Staff Recommendation: Central Staff recommends approval of this action.

ROUTE 7, KEARNY DRAINAGE IMPROVEMENTS INITIAL FINANCIAL PLAN

Town of Kearny, Hudson County and Borough of North
Arlington, Bergen County, New Jersey



Submitted by:

New Jersey Department of Transportation

February 2023

Route 7, Kearny Drainage Improvements

Initial Financial Plan

LETTER OF CERTIFICATION

The New Jersey Department of Transportation (NJDOT) has developed a comprehensive Financial Plan for the Route 7, Kearny Drainage Improvements project in Hudson and Bergen Counties, NJ in accordance with the requirements of Title 23, United States Code, Section 106 and the Financial Plan Guidance issued by the Federal Highway Administration. The plan provides detailed cost estimates to complete the project and estimates of the financial resources necessary to fund the project.

The cost data in the Financial Plan provides an accurate accounting of costs incurred to date and includes an estimate of future costs based on engineer's estimates and anticipated construction cost escalation factors. While the estimates of financial resources rely upon assumptions regarding future economic conditions and demographic variables, these represent realistic estimates of financial resources available to fund the project as described.

NJDOT believes the Financial Plan provides an accurate basis upon which to schedule and fund the Route 7, Kearny Drainage Improvements Project and commits to provide Annual Updates according to the schedule outlined in this Initial Financial Plan.

To the best of our knowledge and belief, the Financial Plan, as submitted herewith, fairly and accurately presents the financial position of the Route 7, Kearny Drainage Improvements Project, cash flows and expected conditions for the project's life cycle. The financial forecasts in the Financial Plan are based on our judgment of the expected project conditions and our expected course of action. We believe that the underlying assumptions of the Financial Plan are reasonable and appropriate. Further, we have made available all significant information we believe is relevant to the Financial Plan and to the best of our knowledge and belief, the documents and records supporting the assumptions are appropriate.

Respectfully submitted:

Charles Maciejunes

Director

Budget: Enterprise Management Office

Date

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Summary

The Route 7 roadway is subject to frequent flooding, which leads to roadway closures, compromises safety for road users, disrupts traffic flow, and contributes to pavement deterioration. Flooding along Route 7 is mainly due to the lack of adequate roadway drainage systems along the corridor, tidal conditions of the Hackensack River, and the impact of the tidal marshland located adjacent to the roadway. The Route 7, Kearny Drainage Improvements project includes raising the roadway profile of Route 7 by approximately 3.5 feet; installation of steel sheeting, three pump stations, and additional inlets; modification of the approach slabs on the Route 7 Bridge over NJ Transit; and electrical modifications to the Route 7 and Porete Avenue signalized intersection.

Title 23, Section 106(i) of the United States Code requires that a recipient of Federal financial assistance for a project with an estimated total cost of \$100 Million or more shall prepare an annual Financial Plan for the project to the United States Secretary of Transportation. A Financial Plan is a comprehensive document that reflects the project's scope, schedule, cost estimate, and funding structure to provide reasonable assurance that there will be sufficient funding available to implement and complete the entire project.

The current estimated cost of the Route 7, Kearny Drainage Improvements project is \$128.977 Million in year of expenditure dollars. This cost estimate includes prior design costs, construction items, utility costs, and Right-of-Way costs.

The Financial Plan includes the following topics as outlined in the Table of Contents:

1. Background
2. Cost Estimate
3. Implementation Plan
4. Project Financing
5. Public Private Partnership Delivery
6. Risk Identification and Mitigation Factors
7. Annual Updates

1.0 Background

1.1 Project Description

The purpose of this project is to address existing drainage issues and flooding problems along Route 7 between Mileposts 1.70 and 3.80. Historical records show that Route 7 frequently floods with as much as 3 feet of water on the roadway. Route 7 floods during rainstorms that coincide with high tides; the roadway also floods during high tides without rainfall since the elevation of the roadway is below the 1-year tidal storm surge elevation in several locations. Existing storm drainage facilities do not meet current NJDOT Design Standards. Results of the roadway stormwater spread analysis show that the spread fails to meet NJDOT standards for a 10-year storm event between Mileposts 1.70 and 3.80.

Proposed improvements include raising the roadway profile by a maximum of 3 feet where feasible. Stormwater spread on the roadway will be controlled for the 10-year storm event. Steel sheeting with a cap elevation of 6.5 feet at locations where the roadway profile elevation is restricted by substandard vertical clearances of existing structures will be installed to prevent tidal storm surge from flooding the roadway. Areas where installation of steel sheeting is not practicable, such as across driveways or under bridge structures, will be protected by earthen berms or barrier curbs.

Three pump stations will be installed where proposed drainage system elevations are below those necessary to achieve gravity flow conditions, including a pump station in the vicinity of the Route 7 Bridge over NJ Transit (Structure No. 0910-154), where the vertical profile has already been established to be above the 2-year tidal storm surge elevation, but below the 10-year elevation. In these critical locations, additional inlets will be installed to expedite removal of stormwater from the roadway during hurricane events like Superstorm Sandy and Hurricane Irene. Multiple drainage system outfalls will be extended, modified, or reconstructed, and six new storm drainage outfalls will be constructed. Manufactured treatment devices will be used to provide water quality treatment of stormwater prior to entering the pump stations. These improvements will mitigate flooding of Route 7 for the 25-year frequency storm event during tidal surges of up to 6.6 feet high, which is the 10-year tidal storm surge elevation.

Additional improvements include modification of the approach slabs of the Route 7 Bridge over NJ Transit to increase the roadway profile above the 10-year tidal storm surge elevation. Modification of the approach slabs will also improve the existing vertical sag curve design speed from 25 mph to 40 mph and 45 mph on the northbound and southbound approaches, respectively. Guide rail will be relocated and/or replaced. Electrical modifications will be made at the Route 7 and Porete signalized intersection, and the flashing curve assembly sign at Milepost 3.85 will be replaced. Modified traffic signal signs and curve warning signs will be installed.

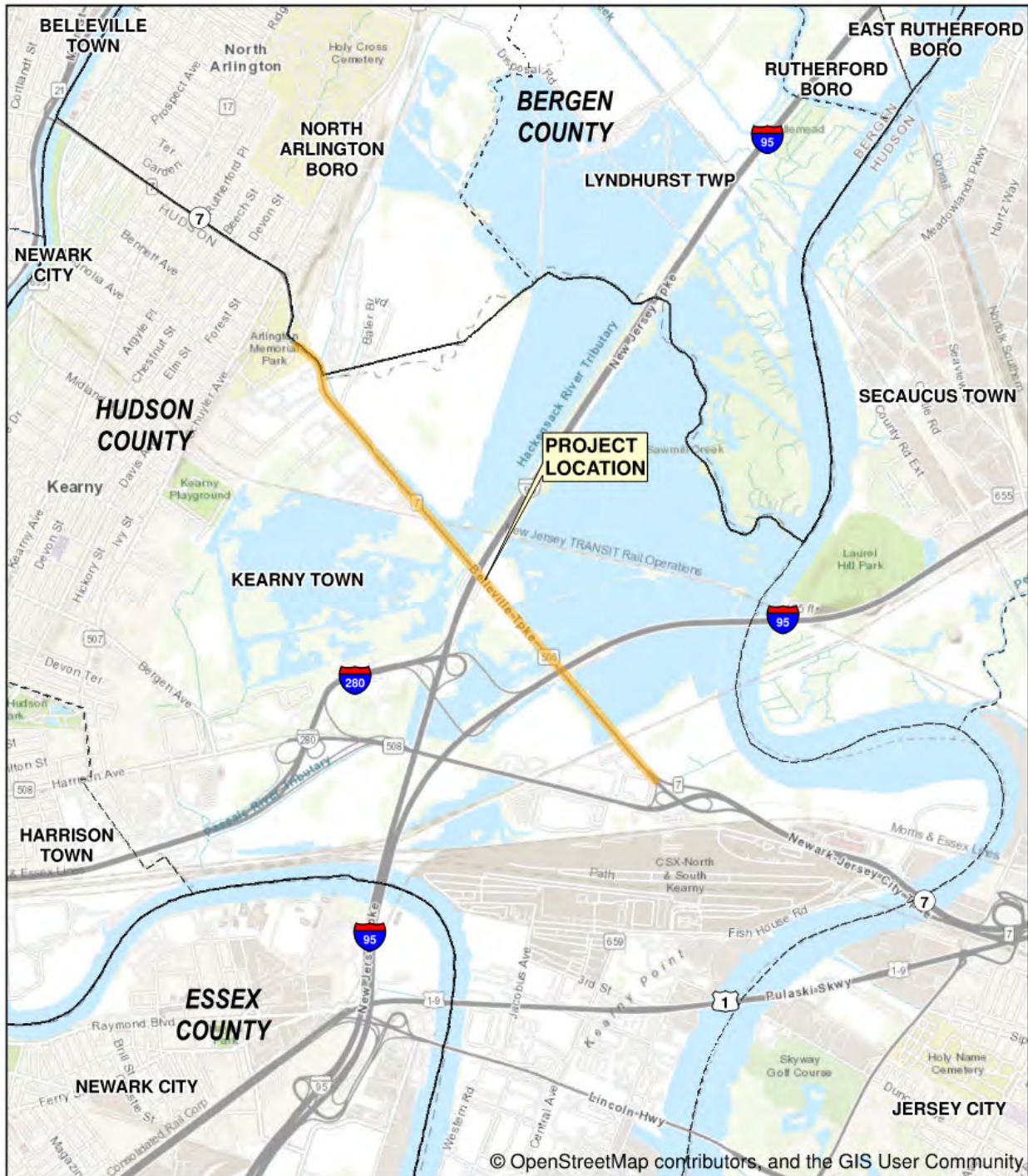


Figure 1-1: Project Location Map

1.2 Project History

Tier 2 Screening/Concept Development

The Route 7, Kearny Drainage Improvements project was initiated through a problem statement that identified flooding between Milepost 1.70 and 3.66 (Sellers Street intersection). The problem statement identified the following:

- A combination of pavement elevations below tidal flood levels and inadequate storm drainage contribute to flooding on Route 7.
- Flooding locations include the AMTRAK overpass at Milepost 1.83; both ends of the bridge over Conrail Road at Mileposts 2.3 and 2.51 and of the bridge over NJ Transit Boonton Line at Mileposts 2.8 and 3.0; the driveway to 700 Belleville Turnpike at Milepost 3.35 and the Conrail Railroad overpass at Milepost 3.62.
- Route 7 has been closed due to flooding approximately 20 times during the past 13 years and the maximum reported water depth on the road was approximately 3 feet.

A Tier 2 Screening was conducted in the mid-2000s. Pursuant to the Tier 2 Screening, alternatives for mitigation of the flooding conditions were prepared by Consoer Townsend Envirodyne Engineers, Inc. and presented to NJDOT in 2005. The project was put on hold, and McCormick Taylor was requested in 2011 to perform a detailed analysis of the Preliminary Preferred Alternative and two other alternatives. At that time, Route 7 was ranked #41 on the NJDOT Drainage Management System (2012); records indicate that Route 7 was flooded 23 times in the year 2010. The roadway was closed for three consecutive days during Hurricane Irene in 2011, and there was evidence of extreme flooding during Superstorm Sandy in 2012.

McCormick Taylor completed the Concept Development Study in September 2012. The PPA included raising the roadway profile 2.5 feet and installing sheeting along the roadway where raising the roadway was not practical. Three pump stations were also proposed.

Preliminary Engineering

Preliminary Engineering (PE) was initiated for the project in August 2013. During Preliminary Engineering, the Preliminary Preferred Alternative was revised to include raising the roadway profile by 3.5 feet where feasible to control stormwater spread for the 25-year storm event. Three stormwater pump stations and installation of barrier curb, earthen berms, or steel sheeting with a cap elevation of 6.5 feet were proposed. Several drainage system outfalls were extended, modified, or reconstructed, and new drainage system outfalls were proposed including three from the pump station. Manufactured treatment devices were proposed to treat for water quality. Additional inlets will be installed to expedite removal of stormwater from the roadway to mitigate flooding for the 25-year storm event. The approach spans for the Route 7 Bridge over NJ Transit Boonton Line (Structure No. 0910-154) Bridge will be replaced with earth fill retained by Mechanically Stabilized Earth walls to support the raise in the roadway profile.

The Categorical Exclusion Document was approved by NJDOT in June 2017 and was approved by FHWA in February 2018. The Preliminary Engineering Report and Plans were finalized in March 2018.

Final Design

Final Design for the project was initiated in May 2019. During Final Design, the design was refined to include raising the roadway profile by 3 feet where feasible and installing three stormwater pump stations. Steel sheeting with a cap elevation of 6.5 feet at locations where the roadway profile elevation is restricted by substandard vertical clearances of existing structures will be installed. Additional inlets will be installed to expedite removal of stormwater from the roadway during hurricane events; multiple drainage system outfalls will be extended, modified, or reconstructed; and six new storm drainage outfalls will be constructed. Manufactured treatment devices will be used to provide water quality treatment of stormwater prior to entering the pump stations. Guide rail will be relocated and/or replaced, electrical modifications will be made at the Route 7 and Porete signalized intersection, and the flashing curve assembly sign at MP 3.85 will be replaced.

The Final Design Submission was made on December 13, 2022. The PS&E Submission is anticipated for February 24, 2023.

Environmental

Although the roadway profile will be raised by 3 feet, there are no sensitive receptors within 200 feet of the project area. The majority of the project area consists mostly of tidal marsh lands on both sides of Route 7. No increase in travel speeds or volumes is anticipated. There are no federally listed species present in the project area.

The project will result in over 1 acre of land disturbance and result in a net increase of 0.62 acres of impervious area; therefore, it is classified as a Major Development and will comply with the NJDEP Stormwater Management Rules (NJ Administrative Code 7:8). Proposed improvements will occur in the immediate vicinity or within the 100-year floodplain of a Passaic River unnamed tributary; however, impacts to the floodplain will be minor. Tidal wetlands and their associated State open waters will be disturbed by the project and mitigation will be required.

Several architectural resources are located within the project's Area of Potential Effects. It was determined that the project will result in No Effect to significant Architectural Resources as long as the NJ Historic Preservation Office is consulted with regards to the appearance and size of the roadway barrier walls.

Concurrent Projects

The following two concurrent projects are located in the vicinity of the Route 7, Kearny Drainage Improvement Project: Koppers Coke Redevelopment (Morris Kearny Associates) and the Portal North Bridge Replacement Project (NJ Transit/Amtrak).

The Koppers Coke project will redevelop a 130-acre site between Route 7 and the Hackensack River to include warehouses and a micro-grid to provide backup power for NJ Transit. Construction is scheduled to begin in Spring 2023. Roadway improvements include upgrading the Route 7 and Cross Pike Drive intersection to provide access to the proposed development. Improvements include installation of a traffic signal, widening of the intersection to include a left turn lane from Route 7 SB into the development, installation of exclusive right turn and left turn lanes on Route 7 NB, and reconstruction of the drainage systems at the intersection. These proposed improvements will impact the southern end of the Route 7, Kearny Drainage Improvements project. Therefore, the construction sequencing was developed to ensure

that construction of the Route 7, Kearny Drainage Improvements project will begin at the northern end of Route 7 and with the pump stations to avoid conflicts with the Koppers Coke construction.

The Portal North Bridge replacement project will construct a new fixed-span bridge to carry the Northeast Corridor over the Hackensack River to replace the existing swing bridge. The new Portal North Bridge and bridge approaches will provide 50-foot vertical clearance over the Hackensack River, thereby necessitating the replacement of the existing railroad bridge over Route 7. The new railroad bridge over Route 7 will be constructed parallel to the existing bridge. Construction of this project has begun and will also require utility relocations along Route 7. Construction access will also be provided from Route 7 and will be coordinated with the construction of the Route 7, Kearny Drainage Improvements project.

1.3 Current Project Activities

Contract Documents

The PS&E Submission for the Route 7, Kearny Drainage Improvements project is anticipated for February 24, 2023.

Environmental Permits

The following environmental permits/approvals were approved:

Environmental Permit	Approval Date
NJDEP Waterfront Development Individual Permit	10/6/2022
NJDEP Flood Hazard Area Individual Permit	10/6/2022
NJDEP Freshwater Wetlands Individual Permit	10/6/2022
Meadowlands District Water Quality Certificate	10/6/2022
US Army Corps of Engineers Section 404 Nationwide Permit	11/8/2022

Right-of-Way Acquisition

Right-of-Way acquisition commenced in January 2022 and is expected to be completed by December 2023. A summary of Right-of-Way parcels for the project is provided below.

Parcel Description	Number of Parcels
Riparian Parcels	2
Partial Fee Takes	15
Easement Parcels	56
Licenses to Cross (New Jersey Turnpike Authority)	2
Business Displacements	0
Residential Displacements	0

Utility Relocation Agreements

Utility Agreement Plans and Modification Agreements have been processed for the project. The following utility companies have facilities within the project limits:

Utility Company
PSE&G (Gas)
PSE&G (Electric)
Verizon-NJ (Telephone)
Comcast Communications Management, LLC (Cable)
Town of Kearny (Water)
Town of Kearny (Sewer)
Bayonne Municipal Utilities Authority (Water)
Jersey City Municipal Utilities Authority (Water)

Community Involvement

A Local Officials Briefing was held with Town of Kearny officials on May 31, 2012, during the Preliminary Engineering phase. Local officials were in favor of the project and provided a Resolution of Support on June 12, 2012. A Public Information Center was also held during the Preliminary Engineering phase on June 28, 2012. Attendees were in favor of the proposed improvements. Community outreach will continue through the construction phase to keep motorists aware of pending construction and changing traffic patterns.

Construction Phase

Construction of the project is expected to begin in October 2023. There will be one construction contract for the entire project. It is anticipated that construction will be substantially complete in February 2029. Full completion of construction is anticipated for July 2029.

2.0 Cost Estimate

2.1 Project Cost Estimate

The current estimated cost of the Route 7, Kearny Drainage Improvements project is \$128.977 Million in year-of-expenditure dollars. **Figure 2-1** below is a graphical representation of the Cost Summary for the Route 7 project for the Initial Cost Estimate, Expenditures to Date, and Cost to Complete. The cost estimate includes all the completed work to date as summarized in **Table 2-1**.

The estimated costs include the overall costs to build including engineering, Right-of-Way, Public Involvement, Construction Inspection, and Construction. The preliminary design estimates have been escalated to year-of-expenditure dollars. NJDOT will annually compare the actual costs incurred to the project cost estimate and adjust the funding allocations if needed to ensure that future costs are reasonable and adequately funded.

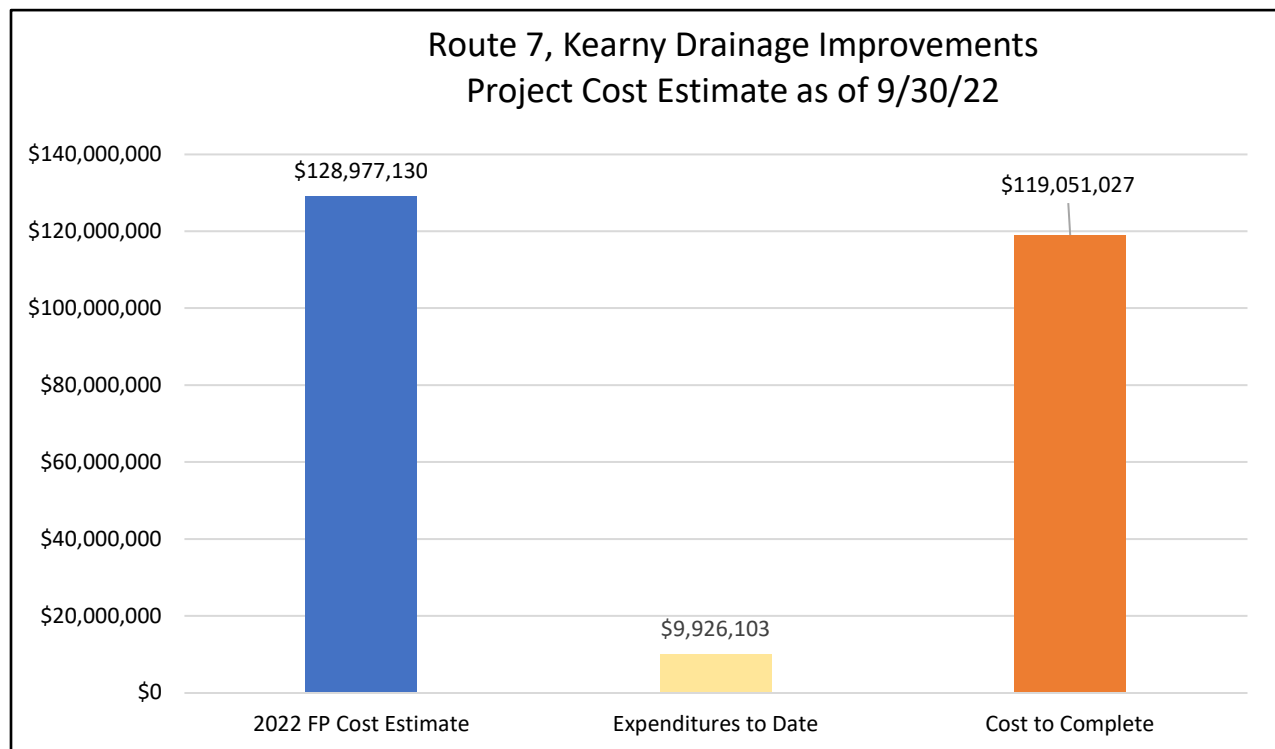


Figure 2-1 Cost Estimate Summary Chart

**Table 2-1 Cost Estimate Summary
(As of 9/30/22)**

Phase	2022 Initial FP Cost Estimate	Expenditures to Date (as of 9/30/2022)	Cost To Complete
Pre-Construction Activities			
Preliminary Engineering	\$3,380,307	\$3,380,307	\$0
Final Design	\$7,205,158	\$6,545,896	\$659,262
ROW Acquisition	\$3,383,000	\$0	\$3,383,000
Subtotal	\$13,968,365	\$9,926,103	\$4,042,262
Construction Activities			
Utility Relocations	\$10,550,000	\$0	\$10,550,000
Construction	\$104,458,765	\$0	\$104,458,765
Subtotal	\$115,008,765	\$0	\$115,008,765
TOTAL PROJECT	\$128,977,130	\$9,926,103	\$119,051,027

2.2 Methodology

The cost estimate for the project was prepared using NJDOT format and methodology, which includes the following major elements of a typical construction project plus project-specific elements added as appropriate.

Preliminary Engineering

These costs were calculated from the actual costs from the Contract Modifications for Preliminary Engineering.

Final Design

Final Design costs were calculated from the actual costs from the Contract Modifications for Final Design.

Right-of-Way Acquisition

The Right-of-Way acquisition cost is based on Right-of-Way funding allocated for the project. All necessary funding for Right-of-Way acquisition has been secured.

Construction Cost

The construction cost estimate was completed using bid cost data provided in the AASHTOWare Project Cost Estimating System, which is NJDOT's primary tool for construction cost estimating. Unit costs from the AASHTOWare Project Cost Estimating System were reviewed and adjusted based on the project size, location, and site-specific conditions.

Utility Relocations

The estimated cost for utility relocations is based on the final utility relocation schemes for the project.

Construction Inspection

The estimated cost is based on the assumption that 10% of the construction cost will be required for construction inspection.

Construction Support Services

The estimated cost is based on the assumption that 8% of the construction cost will be required for construction support services.

2.3 Cost Containment Strategies

NJDOT will consider cost containment strategies that may include, but are not limited to, the following:

- Liquidated Damages Clauses
- Review of project risk based upon documented assumptions

Documented practices for cost forecasting and detailed budget monitoring, combined with reasonable contingencies will be utilized to keep the project costs on target.

3.0 Implementation Plan

3.1 Project Implementation Schedule

The Project Implementation Schedule depicting the project history and anticipated milestones is shown in **Figure 3-1**. The project is expected to be authorized for construction in 2023. Construction is anticipated to begin in Fall 2023 and last approximately 64 months, with a substantial completion date of February 2029. There will be one construction contract for the entire project. The project construction schedule was prepared using Primavera software and in accordance with the *NJDOT Construction Scheduling Manual, 2013*. The construction schedule is included in **Appendix B**.

To achieve the project schedule, the following methods have been or will be implemented:

- Advanced utility relocations
- Advanced ROW acquisition
- Electronic Shop Drawing Review

Task	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	2029	
Preliminary Engineering	Start: 8/2013 End: 8/2019																	
Final Design								Start: 6/2019 End: 2/2023										
ROW Acquisition										Start: 1/2022 End: 12/2023								
Construction												Start: 10/2023 End: 7/2029						

Figure 3-1: Project Implementation Schedule

3.1.1 Project Implementation Schedule Breakdown

The project will require the use of several different crew types to handle the various types of construction operations needed for the project. Specialized work crews will be required for operations such as Pump Station Mechanical, Pump Station Buildings, and Pump Station Electrical Controls/Lighting.

Advanced Utility and Pump Station Work

Utility relocations were broken out in advance of the work zones where feasible. Advanced utility work includes extending the gas main to service Pump Stations 1 and 2, electric relocations in two work zones, lining of the Jersey City MUA water main, ITS construction, and electric relocations to provide services to Pump Stations 1, 2, and 3.

Construction of the three pump stations is also broken out in advance since this work will occur off the roadway and can be constructed without major impacts to the traffic control setup on Route 7. It is anticipated that all three pump stations will be built simultaneously in sequence.

Work Zones

Construction of the project will be broken into the following seven work zones: Railroad Bridge Work Zone, Work Zone 1, Work Zone 2, Work Zone 3, Work Zone 4, Work Zone 5, and Work Zone 6. The work zones were determined based on where the proposed roadway profile meets the existing profile to allow the raised profile to tie in easily between work zones. The schedule assumes that two work zones will be under construction simultaneously; however, the work will be performed in non-adjacent work zones.

Flagging operations are expected for placement of Maintenance & Protection of Traffic, intersection, driveway, drainage, and utility work. The proposed staging anticipates that work can be completed while still maintaining two lanes of traffic or using alternating traffic where necessary; however, a suggested detour route has also been provided in the event that full/partial closure is required.

Anticipated work zone construction is briefly summarized below. Each work zone will have eight stages of construction except for the Railroad Bridge Work Zone, which has four construction stages.

Railroad Bridge Work Zone (November 2023 to October 2025)

Construction operations include minor bridge work; utility relocations; pile installation; reconstruction of approach slabs; construction of retaining walls; and pavement resurfacing.

Work Zone 1 (Fall 2023 to Fall 2024)

Construction operations include guiderail removal/construction; utility pole relocations; drainage construction; inlet reconstruction; construction of a new pavement box; embankment reconstruction; construction of driveways, curbs, and concrete barrier; pavement milling/resurfacing; and signing and striping.

Work Zone 2 (Fall 2024 to Spring 2026)

Construction operations include guiderail removal/construction, drainage construction, pavement milling/resurfacing; embankment reconstruction; utility relocations; construction of a new pavement box; construction of curbs; inlet reconstruction; barrier curb construction with moment slab; and signing and striping.

Work Zone 4 (Fall 2025 to Fall 2026)

Construction operations include guiderail removal/construction; utility relocations; drainage construction; pavement milling/resurfacing; construction of a new pavement box; curb and driveway construction; embankment reconstruction; inlet reconstruction; construction of barrier curb with moment slab; and signing and striping.

Work Zone 6 (Spring 2026 to Spring 2027)

Construction operations include guiderail removal/construction; traffic signal replacement; installation of ITS CCTV camera; retaining wall construction; drainage construction; construction of a new pavement box; embankment reconstruction; utility relocations; driveway, curb, and sidewalk construction; inlet reconstruction; fence installation; and signing and striping.

Work Zone 3 (Fall 2026 to Fall 2027)

Construction operations include guiderail removal/construction; utility relocations; retaining wall construction; drainage construction; pavement milling/resurfacing; construction of a new pavement box; curb and driveway construction; embankment reconstruction; inlet reconstruction; construction of barrier curb with moment slab; and signing and striping.

Work Zone 5 (Fall 2027 to Winter 2029)

Construction operations include guiderail removal/construction; construction of a new pavement box; water main lining; utility relocations; retaining wall construction; drainage construction; access structure construction; pavement milling/resurfacing; embankment reconstruction; curb, driveway, and sidewalk construction; inlet reconstruction; and signing and striping.

Winter Season Work

Pavement reconstruction and/or placement of concrete will be restricted within the winter shutdowns. Work that will be completed during the winter months includes the advanced utility work mentioned above in addition to relocations of Verizon telephone, Bayonne water, PSE&G electric, Comcast, and PS&EG Gas.

4.0 Project Financing

4.1 Overall Financial Plan

As previously illustrated in **Figure 2-1**, the project requires funding of \$128.977 Million, including design costs totaling \$13.968 Million and Right-of-Way costs totaling \$3.383 Million, which have been previously allocated to the project.

The current Fiscal Year (FY) 2022-2031 Statewide Transportation Improvement Program (STIP) includes \$82.7 Million over three fiscal years in additional authorized construction cost for the project. The current STIP funding is shown in **Table 4-1** and is included in **Appendix C**.

Table 4-1: Current Project Funding (FY 2022-2031 STIP)

Phase	Funding Source	FY 2023	FY 2024	FY 2025
Construction	NHPP	\$25.000 M	\$25.000 M	\$32.700 M

A TIP Amendment for the project will be presented to NJTPA in March 2023 that includes the following:

- Adds \$26.0 Million Infrastructure for Rebuilding America (INFRA) grant for construction in FY 2023
- Advances multi-year NHPP funding for Construction in the amount of \$25 Million from FFY 2024 to FFY 2023
- Advances multi-year NHPP funding for Construction in the amount of \$32.7 Million from FFY 2025 to FFY 2023
- Reduces NHPP funding for Construction in FFY 223 from \$82.7 Million by \$0.673 Million to \$82.027 Million
- Adds \$7 Million of State funding in FY 2023 for Construction

The proposed TIP modification will allocate the entire Construction funding amount of \$115.027 Million in FFY 2023. Construction on the Route 7, Kearny Drainage Improvements project is expected to begin in FFY 2023 and extend through the end of FFY 2029. Any yearly modifications in the STIP/TIP will be reflected in the yearly updates to this Financial Plan.

The USDOT INFRA grant program, known statutorily as the Nationally Significant Multimodal Freight and Highway Projects program, awards competitive grants or multimodal freight and highway projects of national or regional significance to improve the safety, efficiency, and reliability of the movement of freight and people in and across rural and urban areas¹. U.S. Senators Bob Menendez and Cory Booker (both D-N.J.) and Congressman Bill Pascrell, Jr. (D-N.J.-09) announced on September 12, 2022, that New Jersey was awarded a \$26 Million grant for the Route 7, Kearny Drainage Improvements project. A total of 26 projects throughout the United States were awarded INFRA grants for 2022.

¹ U.S. Department of Transportation, “The INFRA Grants Program”, Last Modified October 19, 2022, <https://www.transportation.gov/grants/infra-grants-program>.

4.1 Forecasting Project Cost Compared to Allocations by Fiscal Year

The funding for future Fiscal Years in the FY 2022-2031 STIP is programmed for FY 2023-2025, and the proposed TIP modification will re-allocate the entire construction funding of \$115.027 Million in FY 2023. Construction of the project is expected to begin in FFY 2024 and extend through the end of FFY 2029. Through the annual 10-year program update, funding allocations will be adjusted to attempt to match projected fiscal year expenditures with annual allocations.

Figure 6-1 illustrates a summary of the projected expenditures versus allocations by fiscal year. The Cash Flow Table for the project in **Figure 6-2**. As illustrated in the figures, the funding for construction will be fully allocated in FY 2023 based on the proposed TIP modification. However, future funding allocations will need to be adjusted in the STIP to account for projected expenditures in FFY 2024 through FFY 2029.

Total expected expenditures for the project (including Design, ROW, and Construction) are \$128.977 M. Available funding with the proposed TIP Modification will be equal to \$128.995 M.

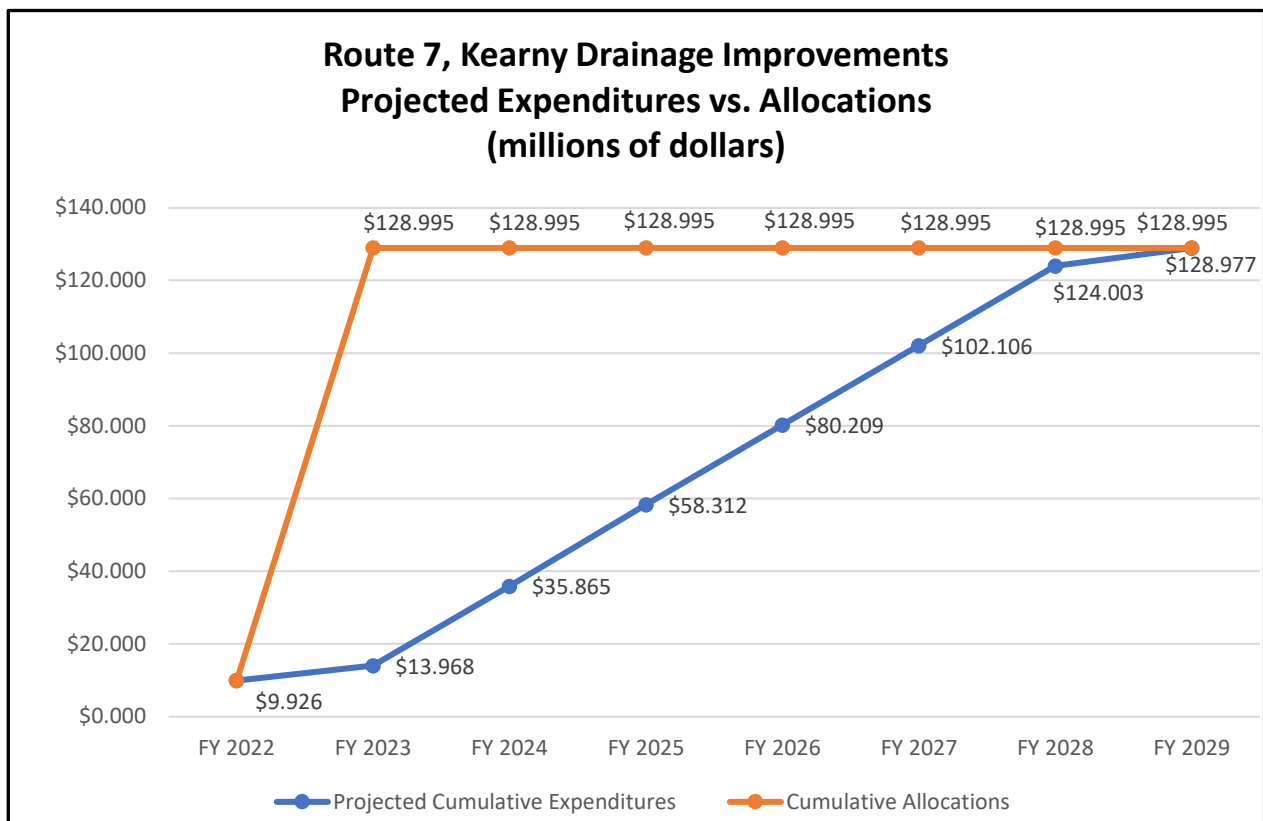


Figure 6-1 Projected Expenditures vs. Allocations

Route 7, Kearny Drainage Improvements
INITIAL FINANCIAL PLAN

Route 7, Kearny Drainage Improvements
As of September 30, 2022
CASH FLOW TABLE
(\$ in Millions)

ITEM	Approved up to FY 2022	Federal FY 2023	Federal FY 2024	Federal FY 2025	Federal FY 2026	Federal FY 2027	Federal FY 2028	Federal FY 2029	Total Estimated Resources
Construction		\$89.626							\$89.626
Utility Construction		\$10.550							\$10.550
Consultant (CES and CM/CI)		\$14.851							\$14.851
Consultant Design	\$9.926	\$0.659							\$10.585
ROW		\$3.383							\$3.383
SUBTOTAL - FUNDING ALLOCATION	\$9.926	\$119.069							\$128.995
TOTAL FUNDING AVAILABLE (CUMULATIVE)	\$9.926	\$128.995	\$128.995	\$128.995	\$128.995	\$128.995	\$128.995	\$128.995	\$128.995

ITEM	Incurred through 9/30/22	Federal FY 2023	Federal FY 2024	Federal FY 2025	Federal FY 2026	Federal FY 2027	Federal FY 2028	Federal FY 2029	Total Estimated Cost
Construction			\$17.068	\$17.068	\$17.068	\$17.068	\$17.068	\$4.267	\$89.608
Utility Construction			\$2.000	\$2.550	\$2.000	\$2.000	\$2.000		\$10.550
Consultant (CES and CM/CI)			\$2.829	\$2.829	\$2.829	\$2.829	\$2.829	\$0.707	\$14.851
Design (PE and FD)	\$9.926	\$0.659							\$10.585
ROW		\$3.383							\$3.383
SUBTOTAL - PROJECTED EXPENDITURES	\$9.926	\$4.042	\$21.897	\$22.447	\$21.897	\$21.897	\$21.897	\$4.974	\$128.977
TOTAL PROJECTED EXPENDITURES (CUMULATIVE)	\$9.926	\$13.968	\$35.865	\$58.312	\$80.209	\$102.106	\$124.003	\$128.977	\$128.977

CUMULATIVE SURPLUS (DEFICIT)	\$0.000	\$115.027	\$93.130	\$70.683	\$48.786	\$26.889	\$4.992	\$0.018	\$0.018
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5.0 Public Private Partnership Delivery

The Route 7, Kearny Drainage Improvements project does not result in a revenue stream for the State of New Jersey. Revenue streams such as Tax Increment Financing or tolling are not feasible for this roadway. Additionally, the project does not introduce any development potential, so taxing new development potential is also not a feasible option. With a project cost of approximately \$128.977 Million, the required investment compared to the potential rate of return is not lucrative for a public private partnership. The ability to attract investors is very low.

The Route 7, Kearny Drainage Improvements project is not a viable candidate for a public private partnership as the project is significantly advanced through Final Design, there is no potential revenue stream, and tolling is not feasible due to the type and location of the roadway.

6.0 Risk Identification and Mitigation Factors

6.1 Risk Factors

NJDOT's Risk Management Process is intended to result in the effective management of project risks during the entire Capital Project Delivery Process. Risk management is categorized into three parts: risk identification, risk analysis, and action. Key elements of the risk management process are to identify potential risks, analyze the probability of occurrence and the magnitude of impact, and develop options and actions to enhance opportunities and reduce threats. It is also critical to monitor identified and potential risks throughout the project process.

With a project of this magnitude and complexity, there are major risks that may be encountered that could impact the project. These risks as well as potential mitigation measures are described below.

Utility Coordination

Utility risks include the following:

- PSE&G Electric relocations will be performed on shared circuits; therefore, concurrent outages could be restricted and prevent concurrent relocations from being performed.
- The condition of some underground utilities is unknown. Additional improvements or replacement may be required if their condition is worsened by construction impacts. This could result in schedule delays and/or increased construction cost.
- Electric relocations and facilities in close proximity to the proposed sheeting and traffic signal may restrict construction.

Mitigation options for these risks include:

- Conduct continuous coordination with utility companies throughout Final Design and Construction to ensure that the utility relocations remain on schedule
- Implement advance utility relocations to the maximum extent feasible
- Develop a schedule for work by outside parties/contractors. Provide the utility company with ample notice as to when relocations will need to occur so the utility company has adequate time for scheduling and mobilization.

- Develop plan for concurrent electric relocations at the three pump stations, which will likely be constructed at the same time.
- Identify potential concerns about the condition of underground utilities as soon as possible, and initiate coordination with utility companies to confirm if unforeseen utility work is required.

Right-of-Way Availability

This project requires Right-of-Way acquisitions for 30 individual parcels. Risks associated with this process include potential construction delays if Right-of-Way is not available prior to the start of construction.

Mitigation options for this risk include:

- Continue coordination and negotiations with individual property owners during Final Design to ensure that ROW is secured prior to construction authorization.

Traffic Control/Maintenance & Protection of Traffic

The project involves significantly raising the roadway profile, which will result in traffic shifts, lane closures, and a large number of construction stages. Allowable lane closure hours and detour routes are also limited, embankments are steep, and available Right-of-Way is limited. Additionally, there are several concurrent projects in the vicinity such as the NJ Transit Portal Bridge and Koppers Coke projects.

Mitigation options for this risk include:

- Use of alternating traffic during overnight construction
- Contractor use of additional crews for traffic control
- Coordinate construction activities with concurrent projects to ensure that proposed traffic staging and detours are not impacted.
- Utilize portable VMS signs and travel time readers during construction to alert the public about changing traffic patterns, lane closures and travel times.
- Consider the use of electronic and social media to disseminate construction information to the motoring public.
- Minimize significant construction activities during peak travel times

Flooding

Since the project area is susceptible to flooding, flood water may overtop the roadway and walls, leading to construction delays.

Mitigation options for this risk include:

- Contractor will implement effective construction measures to reduce the likelihood of overtopping walls

Construction Change Orders

Construction change orders may increase the overall cost of the project. Additionally, material acquisitions have significantly increased as a result of the COVID-19 pandemic.

Mitigation options for this risk includes:

- Control scope creep and ensure that change orders are necessary
- Identify critical materials and activities that might be affected and prepare contingency plans

6.2 Cost Increase Risks

Financial risks for this project include cost increase risks and revenue sufficiency risks as described below. The construction cost for this project exceeds \$115 Million, and allocated funding for Construction on the STIP and the proposed TIP Modification is \$115.027 Million, which fully funds construction of the project. Any additional increase in construction cost would impact the STIP funding and result in the need to shift funds from other projects or delay construction of this project.

To mitigate the risk of receiving high bids, NJDOT will consider the following mitigation strategy:

- Include contingencies for construction

Additional challenges include supply chain problems that make it difficult to budget for and procure construction materials. The procurement time for electrical items required for the traffic signal and CCTV camera will require up to 120 days. Long lead times may also be required for the specialized mechanical and electrical items required for the pump stations. The PS&EG cast iron gas main replacement will require steel pipe that will be procured by PS&EG. Inflated prices of construction materials have led to increased costs for construction projects throughout New Jersey and the nation. Inflation and supply chain issues have been impacted by many factors including the COVID-19 pandemic. The producer price index for inputs to construction rose more than 7% in 2022, which outpaced the 6.5% rise in the consumer price index (inflation). Costs are expected to continue to rise in 2023. Increases in material costs should be monitored as the project progresses, as construction is expected to continue for more than 5 years.

Unanticipated site conditions also represent a risk for a project of this magnitude. There is also a risk that increasing fuel prices will increase the project cost. To mitigate this risk, the bid proposal can include a lump sum item to provide compensation to the contract holder for fluctuations in the fuel price over the life of the project. Using the award date as the base point, fuel price adjustments can be paid monthly during months when fuel prices change by 5% or more. During periods of decline in the fuel price, the State will be compensated, and during periods of increase, the Contractor will be compensated. Compensation will be made through the use of a weighted formula provided in the specifications.

NJDOT estimates the total fuel price adjustment by multiplying a pro-rated volume of the fuel-intensive construction activities by the duration of the project and increases this cost utilizing 5% inflation per year. Assuming the actual increases stay within the estimate, no additional project costs will be realized. According to the specifications, should the fuel price adjustment exceed 50%, fuel-intensive construction activities are to cease pending approval by the Executive Director of Regional Operations.

6.3 Revenue Sufficiency Risks

Federal funds are the funding source for this project. The existing funding plan is for three years (FY 2023-2025). The proposed TIP modification will add a \$26 Million INFRA grant and \$6.327 Million in additional funding. There are financing risks associated with the current funding plan as described below.

There is the risk that the project cost will increase due to change orders during construction. NJDOT typically funds change orders through supplemental authorizations. Funding change orders through supplemental authorizations may take obligation authority from current programmed unobligated state items.

NJDOT Division of Capital Investment and Program Coordination is responsible for developing the STIP, which will provide funding for this project. The Route 7, Kearny Drainage Improvements project (DB #93186) is included in Plan 2050, which is the North Jersey Transportation Planning Authority's (NJTPA) Long-Range Plan. The project is classified as a Near-Term Road Preservation project and is one of the largest projects in the NJTPA region. Therefore, it is critical that NJDOT and NJTPA work together to ensure that adequate funding is programmed for this project.

7.0 Annual Updates

Annual updates of this Financial Plan will provide information on the actual cost and expenditure performance in comparison to initial estimates. Additionally, the plan will be updated with estimates of future year obligations and expenditures.

Yearly updates to this plan will include updates to the Funding and Cash Flow sections of the plan to reflect actual cost-to-date and cost-to-complete estimates. These estimates will be shown in year of expenditure dollars to be consistent with the initial financial plan document. Any significant changes to the total project cost or schedule since the previous estimate will be identified, and major reasons for these changes will be clearly described. If significant changes are anticipated, actions to monitor project cost and/or project schedule will be identified.

Design, Right-of-Way, and Construction costs for this project are federally funded. Annual updates will be submitted to NJDOT within 90 days following the end of the Federal Fiscal Year.



State of New Jersey

DEPARTMENT OF TRANSPORTATION
1035 Parkway Avenue
PO Box 600
Trenton, New Jersey 08625-0600

MPO: NJTPA

PROJECT: Route 7, Kearny, Drainage Improvements

DBNUM: 93186

TRACK #: N-93186-2-M-2022

TYPE OF PROJECT CHANGE: Project Cost increase

ACTION TAKEN: Advances multi-year NHPP funding for CON in the amount of \$25M from FFY 24 to FFY 23
Advances multi-year NHPP funding for CON in the amount of \$32.7M from FFY 25 to FFY 23
Reduces NHPP funding for CON in FFY 23 from \$82.7M by \$.673M to \$82.027M
Adds INFRA grant of \$26M in FY 23 for CON
Adds \$7M of STATE in FY 23 for CON
Total CON increase of \$32.327M for a total programmed amount of \$115.027M

REASON FOR CHANGE:

Per the PM of the project: The cost was increased almost 34% due the cost increase of materials especially steel after COVID 19, due to inflation and material availability issues. That include steel sheeting, Ductile Iron pipes, Pump stations equipment and other items including utility cost.

MPO ACTION REQUIRED: Action requires MPO board approval

FISCAL CONSTRAINT:

Comments: See NJTPA FY 22-31 Chart 65

CONFORMITY:

PUBLIC INVOLVEMENT:

DRAFT RESOLUTION: APPROVAL OF THE FY 2024 UNIFIED PLANNING WORK PROGRAM

WHEREAS, the North Jersey Transportation Planning Authority, Inc. (NJTPA) has been designated by the Governor of New Jersey as the Metropolitan Planning Organization (MPO) for the northern New Jersey region; and

WHEREAS, the NJTPA, pursuant to 23 U.S.C. 104 (f) et. seq. and 49 U.S.C. 1607 et. seq., is responsible for the development of the Unified Planning Work Program (UPWP) to guide the transportation planning process in northern New Jersey; and

WHEREAS, the Fiscal Year 2024 UPWP (July 1, 2023 to June 30, 2024) describes all urban transportation and transportation-related planning activities to be undertaken by the NJTPA Central Staff, New Jersey Department of Transportation, NJ TRANSIT, NJTPA subregions, Transportation Management Associations, and other planning agencies in the region; and

WHEREAS, the FY 2024 UPWP is fully consistent with the U.S. Department of Transportation Metropolitan Transportation Planning Regulations (23 CFR part 450) which became effective May 27, 2016, as well as the Infrastructure Investment and Jobs Act (IIJA); and

WHEREAS, any FY 2024 Compensation Plan and salary parameters will be subject to the NJTPA Executive Committee approval and New Jersey Institute of Technology confirmation; and

WHEREAS, no action authorized by the NJTPA shall have force or effect until ten (10) days, Saturdays, Sundays and public holidays excepted, after a copy of the minutes of the meeting of the Board of Trustees has been delivered to the Governor for review, unless prior to expiration of the review period the Governor shall approve same, in which case the action shall become effective upon such approval.

NOW, THEREFORE, BE IT RESOLVED, that the North Jersey Transportation Planning Authority hereby approves the FY 2024 Unified Planning Work Program for the region.

BE IT FURTHER RESOLVED, that a copy of this resolution is forwarded to the New Jersey Department of Transportation for submission to the Federal Highway Administration and the Federal Transit Administration.

Summary of Action **Approval of the FY 2024 Unified Planning Work Program**

Action: Approval of the FY 2024 Unified Planning Work Program (UPWP).

Background: The UPWP describes all metropolitan transportation and transportation-related planning activities to be undertaken by NJTPA Central Staff, subregions, member agencies and the Transportation Management Associations during FY 2024 (July 1, 2023 to June 30, 2024). This document has been developed with the direct input of the federal funding agencies, the New Jersey Department of Transportation (NJDOT), NJ TRANSIT, the Port Authority of New York & New Jersey, the NJTPA Board of Trustees and the Regional Transportation Advisory Committee (RTAC). The NJTPA has provided opportunities for the public to review and comment on the UPWP consistent with NJTPA public participation procedures.

Pursuant to federal planning regulations, the NJTPA is required to develop an annual UPWP. The FY 2024 UPWP is guided by the NJTPA Board of Trustees, the planning requirements of the federal Infrastructure Investment and Jobs Act (IIJA) and its precursor legislation, and *Plan 2050: Transportation. People. Opportunity.*, the NJTPA's long range transportation plan. The FY 2024 UPWP incorporates work tasks that reflect current planning priorities of the United States Department of Transportation and NJDOT.

The initial FY 2024 UPWP draft was issued in December 2022 for review. Comments received from members of the Board, subregional partners, federal sponsors and NJDOT have been reviewed and incorporated into the work program and budget where appropriate.

For informational purposes, the work program also includes Chapter IV - Other Regional Transportation Planning Initiatives, which describes other transportation and transportation-related planning activities that are not funded by the NJTPA and not included in the other chapters of the UPWP.

The proposed FY 2024 UPWP is available on the NJTPA website at: <http://www.njtpa.org/upwp.aspx>. Pending Board approval and federal authorization of the FY 2024 UPWP, final copies of the UPWP will be posted on the NJTPA's website for access by the NJTPA Board of Trustees, RTAC, and the general public. A copy of the FY 2024 budget summary and comments received on the December 2022 draft are attached herewith for reference.

Justification for Action: NJTPA Board approval of the UPWP is necessary to ensure receipt of federal funding and for staff activities to be conducted in the upcoming fiscal year.

Staff Recommendation: Central Staff recommends approval of the FY 2024 UPWP.

NJTPA FY 2024 UPWP Budget Summary

Expenditures - FY 2024 UPWP Program Activities	UPWP Budget
Central Staff Program Activities <i>(Chapter I)</i>	
Personnel Expenditures ^{1, 2}	\$ 9,043,600
Non-Personnel Expenditures	\$ 2,344,000
Indirect Costs ³	\$ 1,935,900
Subtotal: Central Staff Activities	\$ 13,323,500
Contractual\Consultant Subcontracts <i>(Chapter I)</i>	
UPWP Consultant Projects	\$ 4,749,000
UPWP Subrecipient Projects	\$ 783,000
Subtotal: Contractual\Consultant Subcontracts	\$ 5,532,000
Pass-through Programs <i>(Chapters II & III)</i>	
FY 2024 Subregional Transportation Planning Program	\$ 2,307,000
FY 2024 STP Supplemental Support	\$ 227,000
FY 2024 - FY 2025 Subregional Studies Program	\$ 1,189,000
FY 2024 Transportation Management Association Program	\$ 6,479,000
Subtotal: Pass-through Program Subcontracts	\$ 10,202,000
Total: FY 2024 UPWP Expenses	\$ 29,057,500

Revenues - FY 2024 UPWP Funding Authorizations ⁴	UPWP Budget
FHWA MPP PL Funds (IIJA FFY22 & FFY23, STIP DB# X30A)	\$ 13,383,089
FHWA MPP Set-aside PL Funds -SATO (IIJA FFY22 & FFY23, STIP DB# X30A)	\$ 513,410
FHWA MPP PL Funds, Reprogrammed (S-LU, MAP-21, FAST Act PL funds)	\$ 5,152,706
Flexed FTA Section 5303/5305(d) MP Program Funds (IIJA 22 MP, STIP DB# X30A)	\$ 732,723
FHWA Surface Transportation Block Grant Program (STBGP-NY/NWK) Funds (FFY23, STIP DB# X30A)	\$ 2,148,296
FHWA STBGP-NY/NWK Funds for TMA Program (FFY23, STIP DB# 11383)	\$ 4,450,000
FHWA STBGP-PHILA Funds for TMA Program (DVRPC TMA, FFY23, STIP DB# 11383)	\$ 1,985,000
Local Share - Subregional Transportation Planning and Subregional Studies Programs	\$ 692,275
Total: FY 2024 UPWP Revenues	\$ 29,057,500

Notes:

(1) The FY 2024 budget assumes the salaries of 58 full-time Central Staff employees, plus part-time staff for additional support. A total of 67.1 full time equivalent (FTE) employees are assumed with part-time staff counted as 0.65 FTE. The FY 2024 budget also assumes a merit pool of 2.75%.

(2) FY24 Budget assumes provisional FY24 fringe benefit rates of 35.1% for full time staff and 8.3% for hourly part time staff, as agreed to by NJIT (the NJTPA's Host Agency) and U.S. Department of Health & Human Services. Actual FY 2024 rates will be subject to audit.

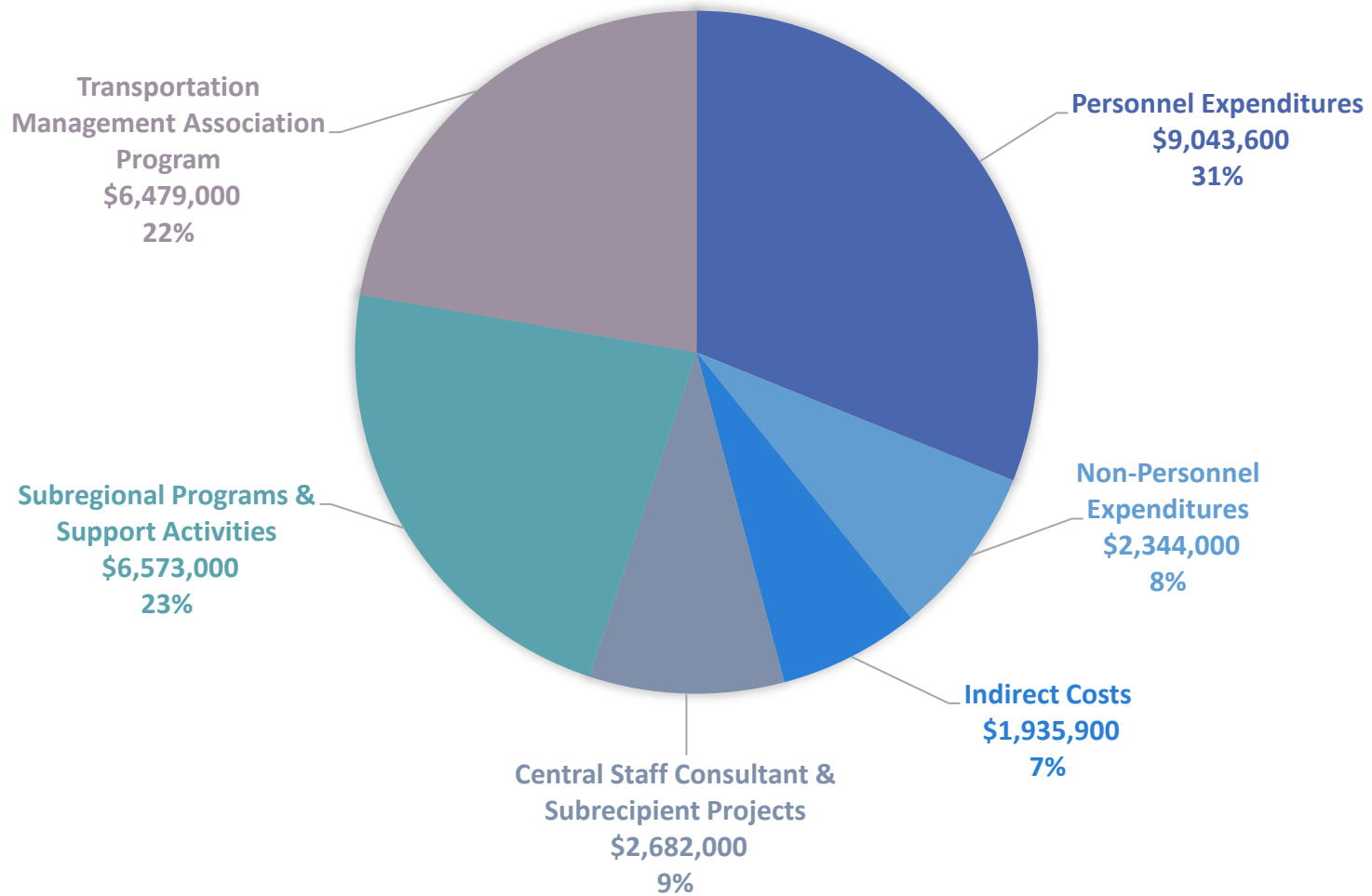
(3) The FY 2024 budget for NJIT's Facilities and Administration Costs assumes a fixed amount, as stated in the current NJDOT Basic Agreement with NJTPA and NJIT (Agreement No. 2021-NJIT-001), calculated from a rate based on past fiscal years' audited costs that is applied to projected FY24 total operating costs.

(4) Funding to be authorized for the FY 2024 program assumes FFY22 and FFY23 FHWA MPP PL funds (IIJA apportionments, including 2.5% of PL funds set-aside for Safe and Accessible Transportation Options); reprogrammed FHWA MPP and Flexed FTA MP PL funds from prior apportionments (unobligated\available S-LU, MAP-21 and FAST Act balances, including funds released from NJTPA Task Orders PL-NJ-19-01, PL-NJ-20-01, PL-NJ-21-01 and PL-NJ-22-01); FTA 22MP PL funds flexed to FHWA; STBGP-NY/NWK and STBGP-PHILA funds (STIP DB Nos. X30A and 11383); and a local share (in-kind match).

(5) For details of the central staff budget expenses, and consultant and subrecipient subcontracts, see pages 10, 11, and 12.

NJTPA FY 2024 UPWP BUDGET

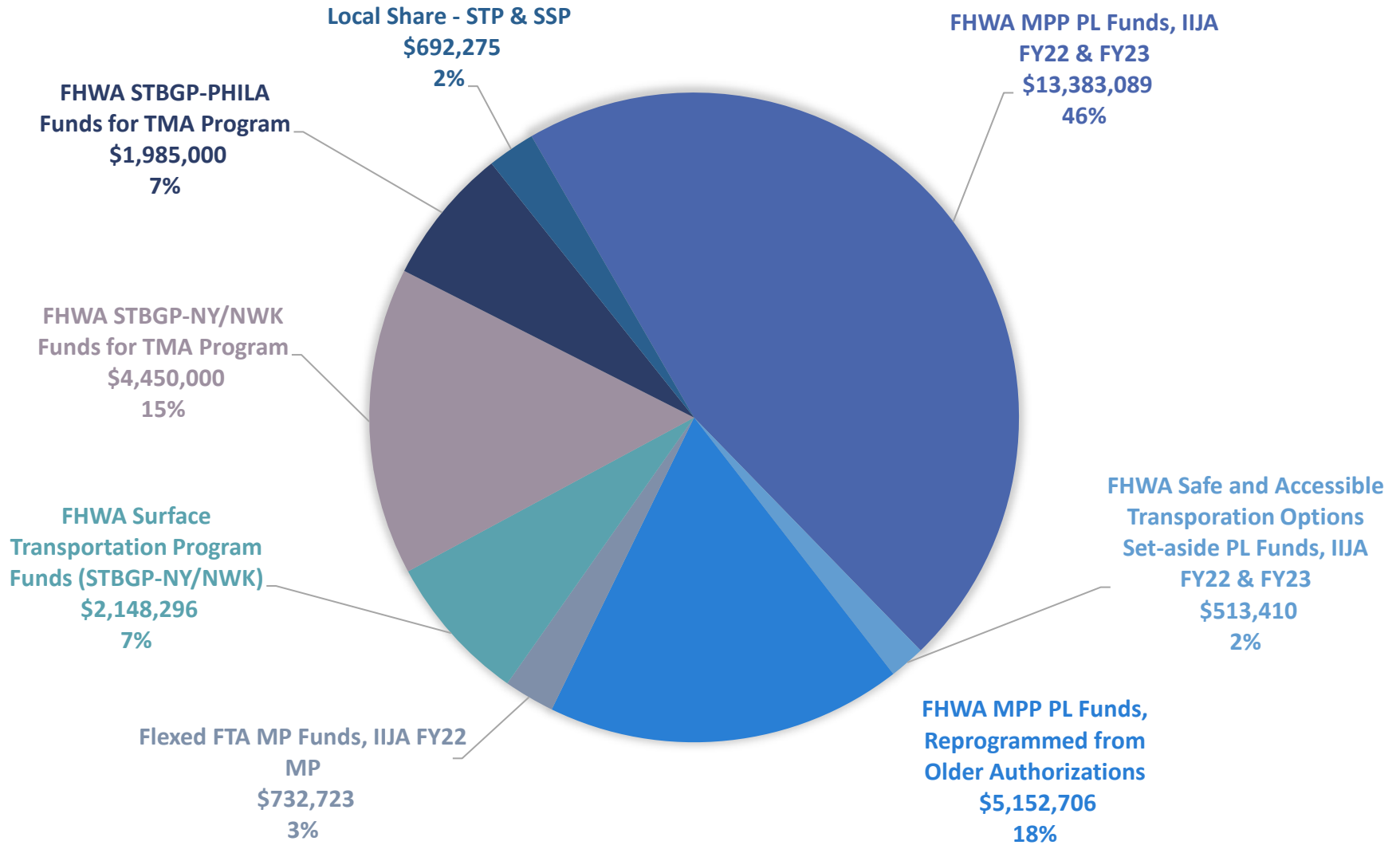
TOTAL EXPENDITURES \$29,057,500



Note: Subregional Support Activities include select Central Staff projects outlined in Chapter I, which provide direct support to local agencies

NJTPA FY 2024 UPWP BUDGET

TOTAL REVENUES \$29,057,500



NJTPA FY 2024 UPWP Budget New Contractual/Consultant Projects

NEW FY 2024 UPWP Projects - Task Order PL-NJ-24-01

Task No.	Task Activity	Contract Budget	Total Project Budget ¹	Effective Funding Period
UPWP Consultant Projects (Chapter I)				
24/203-01	Accessibility and Mobility Regional Reassessment	\$ 250,000	\$ 253,000	7/1/23 - 6/30/25
24/205-01	Air Quality SIP and GHG Inventory Updates	\$ 175,000	\$ 177,000	7/1/23 - 6/30/25
24/303-01	Local Safety Action Plans	\$ 1,650,000	\$ 1,667,000	7/1/23 - 6/30/25
24/401-01	2050 Freight Industry Level Forecasts Update	\$ 550,000	\$ 556,000	7/1/23 - 6/30/25
24/501-01	Criteria Development and Project Scoring Update	\$ 475,000	\$ 480,000	7/1/23 - 6/30/26
24/602-01	FY 2024 Consultant Assistance with LSP - Studies and Analyses	\$ 1,000,000	\$ 1,010,000	7/1/23 - 6/30/26
24/701-01	Long Range Transportation Plan Public Outreach	\$ 300,000	\$ 303,000	7/1/23 - 6/30/26
23/802-01	FY 2024 UPWP Management System Support	\$ 300,000	\$ 303,000	7/1/23 - 6/30/25
Subtotal - UPWP Consultant Projects		\$ 4,700,000	\$ 4,749,000	
UPWP Subrecipient Projects (Chapter I)				
24/202-01	Trans Hudson Bus Survey Phase IV	\$ 250,000	\$ 253,000	7/1/23 - 6/30/24
24/307-01	FY 2024 TNJ Initiative	\$ 225,000	\$ 227,000	7/1/23 - 6/30/24
24/307-02	FY 2024 Complete Streets Technical Assistance	\$ 200,000	\$ 202,000	7/1/23 - 6/30/25
24/701-02	FY 2024 Innovative Public Engagement	\$ 100,000	\$ 101,000	7/1/23 - 6/30/24
Subtotal - UPWP Subrecipient Projects		\$ 775,000	\$ 783,000	
UPWP Pass-Through Programs (Chapters II & III) ²				
24/304-01	FY 2024 Subregional Transportation Planning Program	\$ 2,283,875	\$ 2,307,000	7/1/23 - 6/30/24
24/304-02	FY 2024 STP Supplemental Support	\$ 225,000	\$ 227,000	7/1/23 - 6/30/24
24/304-03	FY 2024 - FY 2025 Subregional Studies Program	\$ 1,177,500	\$ 1,189,000	7/1/23 - 6/30/25
24/305-01	FY 2024 Transportation Management Association Program	\$ 6,415,000	\$ 6,479,000	7/1/23 - 6/30/24
Subtotal - UPWP Pass-Through Programs		\$ 10,101,375	\$ 10,202,000	
Total		\$ 15,576,375	\$ 15,734,000	

Notes:

- (1) Includes 1% NJTPA Administration Fee, to be added to the subcontract costs for the federal budget and funding agreements.
- (2) A detailed list of subcontracts for the pass-through programs are provided on page 11.
- (3) The following Central Staff contractual activities (Chapter I) provide direct support to local agencies: 24/303-01, 24/602-01, and 24/307-03.

**NJTPA FY 2024 UPWP Budget
Pass-Through Programs Subcontracts**

FY 2024 Subregional Transportation Planning Program, Chapter II	Federal Share	Local Share	Total Program Budget	STP Supplemental Support	Effective Funding Period
Bergen County STP Program	\$ 198,164.00	\$ 49,541.00	\$ 247,705.00	\$ 15,000	7/1/23- 6/30/24
Essex County STP Program	\$ 132,966.00	\$ 33,241.50	\$ 166,207.50	\$ 15,000	7/1/23- 6/30/24
Hudson County STP Program	\$ 113,296.00	\$ 28,324.00	\$ 141,620.00	\$ 15,000	7/1/23- 6/30/24
Hunterdon County STP Program	\$ 71,010.00	\$ 17,752.50	\$ 88,762.50	\$ 15,000	7/1/23- 6/30/24
Jersey City STP Program	\$ 90,530.00	\$ 22,632.50	\$ 113,162.50	\$ 15,000	7/1/23- 6/30/24
Middlesex County STP Program	\$ 182,571.00	\$ 45,642.75	\$ 228,213.75	\$ 15,000	7/1/23- 6/30/24
Monmouth County STP Program	\$ 153,190.00	\$ 38,297.50	\$ 191,487.50	\$ 15,000	7/1/23- 6/30/24
Morris County STP Program	\$ 130,583.00	\$ 32,645.75	\$ 163,228.75	\$ 15,000	7/1/23- 6/30/24
City of Newark STP Program	\$ 95,367.00	\$ 23,841.75	\$ 119,208.75	\$ 15,000	7/1/23- 6/30/24
Ocean County STP Program	\$ 144,381.00	\$ 36,095.25	\$ 180,476.25	\$ 15,000	7/1/23- 6/30/24
Passaic County STP Program	\$ 132,048.00	\$ 33,012.00	\$ 165,060.00	\$ 15,000	7/1/23- 6/30/24
Somerset County STP Program	\$ 102,946.00	\$ 25,736.50	\$ 128,682.50	\$ 15,000	7/1/23- 6/30/24
Sussex County STP Program	\$ 74,434.00	\$ 18,608.50	\$ 93,042.50	\$ 15,000	7/1/23- 6/30/24
Union County STP Program	\$ 137,822.00	\$ 34,455.50	\$ 172,277.50	\$ 15,000	7/1/23- 6/30/24
Warren County STP Program	\$ 67,792.00	\$ 16,948.00	\$ 84,740.00	\$ 15,000	7/1/23- 6/30/24
Total STP Program	\$ 1,827,100.00	\$ 456,775.00	\$ 2,283,875.00	\$ 225,000	7/1/23- 6/30/24

In addition, a total maximum budget of \$225,000 has been allocated in the FY 2024 UPWP's STP Program for supplemental support, with a \$15,000 allotment to each subregion (Task 24/304-02 FY 2024 STP Supplemental Support; there is no local share requirement for these funds).

FY 2024 - FY 2025 Subregional Studies Program, Chapter II	Federal Share	Local Share	Total Program Budget	Effective Funding Period
Hudson County: West Hudson Circulation Plan	\$ 300,000.00	\$ 75,000.00	\$ 375,000.00	7/1/23 - 6/30/25
Middlesex County: Middlesex County Road Safety Audit	\$ 450,000.00	\$ 112,500.00	\$ 562,500.00	7/1/23 - 6/30/25
Warren County: Comprehensive Complete Street Policy	\$ 192,000.00	\$ 48,000.00	\$ 240,000.00	7/1/23 - 6/30/25
Total FY 2024-FY 2025 SSP Program	\$ 942,000.00	\$ 235,500.00	\$ 1,177,500.00	7/1/23 - 6/30/25

Total Chapter II - Subregional Programs	\$ 2,769,100.00	\$ 692,275.00	\$ 3,461,375.00
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FY 2024 Transportation Management Association (TMA) Program, Chapter III	Federal Share	Local Share	Total Program Budget	Effective Funding Period
FY24 TMA Work Program: Avenues in Motion	\$ 955,000.00	\$ -	\$ 955,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: Cross County Connection TMA	\$ 1,100,000.00	\$ -	\$ 1,100,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: EZRide	\$ 1,405,000.00	\$ -	\$ 1,405,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: goHunterdon	\$ 460,000.00	\$ -	\$ 460,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: Greater Mercer TMA	\$ 865,000.00	\$ -	\$ 865,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: Hudson TMA	\$ 520,000.00	\$ -	\$ 520,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: Keep Middlesex Moving	\$ 620,000.00	\$ -	\$ 620,000.00	7/1/23- 6/30/24
FY24 TMA Work Program: RideWise	\$ 490,000.00	\$ -	\$ 490,000.00	7/1/23- 6/30/24
Total TMA Program	\$ 6,415,000.00	\$ -	\$ 6,415,000.00	7/1/23- 6/30/24

Total Chapter III - TMA Program	\$ 6,415,000.00	\$ -	\$ 6,415,000.00
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